

WASHINGTON COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*WASH01	J 987	Cedar Creek Bridge	1-120' riveted Pratt through truss 1935 Lay Construction Company
*WASH02	013003.7	Kingston Ford Bridge	1-166' pinned Pratt through truss 1886 St. Louis Bridge and Iron Co.
*WASH03	086004.0	Haefner Bridge	m1917 Miller & Borcharding, St. Louis 2-100' pinned Pratt through truss 1918 R.L. Miller, St. Louis

EXCLUDED:

Pratt pony truss
G 461R

Warren pony truss
153000.8

Steel stringer
S 522 T 114 X 449 X 925 Y 982 032000.0 042000.1
061003.2

Concrete girder
H 271 H 506R J 403R J 988 T 399 T 400 065000.6

Concrete slab
013001.3 018000.1 060001.9 062000.2 071002.8 079005.8 079006.4
081002.0 083001.7 088000.4 089000.0 097000.2 098000.8 102002.0
156000.1 213500.1 351500.4

Concrete box culvert
A 2979 F 836R G 440R G 474R G 475R H 505 J 60
J 150 J 293 J 294 J 404 J 405 J 414 K 52
K 353 L 366 S 820 S1000 T 119 T 123 T 124
W 92 W 217 W 539 X 431 X 432 Y 412 Y 497
Y 498 Y 747 Y 748

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	1	2	0	0	3
Excluded	43	22	0	0	65
	44	24	0	0	68 structures

Cedar Creek Bridge

WASH01

GENERAL DATA

structure no.:	J 987	city/town:	6.3 miles southwest of Irondale
county:	Washington	feature inters.:	Cedar Creek
		cadastral grid:	S7, T35N, R3E
		highway route:	Missouri State Highway 32
		highway distr.:	9
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt through truss, with steel stringer approach spans

substructure: concrete abutments, wingwalls and piers

span number:	1	condition:	good
span length:	120.0'	alterations:	none
total length:	250.0'	floor/decking :	concrete deck over steel stringers
roadway width:	22.0'	other features:	steel angle guardrails

HISTORICAL DATA

erection date: 1934-35

erection cost: \$19,096.25

designer: Missouri State Highway Department

fabricator : Inland Steel Company, East Chicago IN

contractor: Lay Construction Company

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. J 987; Missouri Highway and Transportation Department, Primary System Bridge Record, located at Bridge Division, MHTD, Jefferson City, Missouri; field inspection by Clayton Fraser, 6 September 1991.

sign. rating: 41

evaluation: NRHP non-eligible (typically configured example of MSHD truss design of the 1930s)

Inventoried by: Clayton B. Fraser 17 January 1992

Kingston Ford Bridge

WASH02

GENERAL DATA

structure no.:	013003.7	city/town:	12.5 miles northeast of Potosi
county:	Washington	feature inters.:	Big River
		cadastral grid:	S22, T39N, R3E
		highway route:	county road
		highway distr.:	9
		current owner:	Washington County

STRUCTURAL DATA

superstructure:	wrought iron, 8-panel, pin-connected Pratt through truss with steel stringer approach spans		
substructure:	concrete-filled steel cylinder piers, with concrete abutments and steel pile bent piers at approach spans		
span number:	1	condition:	fair
span length:	166.0'	alterations:	truss moved
total length:	254.0'	floor/decking :	timber deck over steel stringers
roadway width:	14.8'	other features:	upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with unslotted turnbuckle; strut: 2 braced angles; floor beam: I-beam, U-bolted to vertical; 2-channel guardrails

HISTORICAL DATA

erection date: 1885-86 (moved 1917)
erection cost: \$9870.00 (superstructure contract amount)
designer: St. Louis Bridge and Iron Company, St. Louis MO
fabricator : St. Louis Bridge and Iron Company, St. Louis MO
contractor: St. Louis Bridge and Iron Company, St. Louis MO (1885 construction); Miller & Borcharding, St. Louis MO (1917 move)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 013003.7; Jefferson County Court Record 4: page 313 (7 May 1885), page 328 (4 August 1885), page 349 (28 September 1885), page 355 (2 November 1885), page 418 (20 April 1886); Jefferson County Court Record 12: page 411 (20 July 1916), page 443 (23 September 1916), pages 518-20 (19 March 1917), located at Jefferson County Courthouse, Hillsboro MO; Washington County Court Record K: page 321 (1 May 1916), page 333 (4 May 1916), page 349 (30 June 1916), pages 371-72 (12 August 1916), page 396 (14 November 1917), page 403 (18 November 1917); Washington County Court Record L: page 24 (5 April 1917), located at Washington County Courthouse, Potosi MO; field inspection by Clayton Fraser, 6 September 1991.

Haefner Bridge

WASH03

GENERAL DATA

structure no.: 086004.0 city/town: 5.0 miles southwest of Irondale
county: Washington feature inters.: Cedar Creek
cadastral grid: S5, T35N, R3E
highway route: county road
highway distr.: 9
current owner: Washington County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss
substructure: concrete abutments, wingwalls and pier

span number: 2 condition: fair
span length: 100.0' alterations: none
total length: 202.0' floor/decking : timber deck over steel stringers
roadway width: 13.3' other features: upper chord/ inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with knee braces; portal strut: A-frame; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: 1918
erection cost: \$4950.00
designer: R.L. Miller, St. Louis MO
fabricator : unknown
contractor: R.L. Miller, St. Louis MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 086004.0; Washington County Court Record L: page 168 (5 July 1918), page 171 (5 July 1918), page 186 (2 September 1918), located at Washington County Courthouse, Potosi MO; "Washington County Missouri" [map], Potosi, MO: published by W.T. Hunter and M.E. Rhodes, 1885; field inspection by Clayton Fraser, 6 September 1991.

sign. rating: 39
evaluation: NRHP non-eligible (typically configured example of common structural type)

Inventoried by: Clayton B. Fraser 17 January 1992

Kingston Ford Bridge

sign. rating: 56

evaluation: NRHP possibly eligible (well-preserved, long-span example of early wrought iron truss construction)

inventoried by: Clayton B. Fraser 17 January 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Kingston Ford Bridge (Big River Bridge)
MHTD: 013003.7

WASH02

DATE(S) OF CONSTRUCTION

1885-86 (moved 1917)

LOCATION

county road over Big River; S22, T39N, R3E
12.5 miles northeast of Potosi; Washington County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP potentially eligible (score: 56)

CONDITION

fair

OWNER

Washington County

span number: 1
span length: 166.0'
total length: 254.0'
roadway wdt.: 14.8'

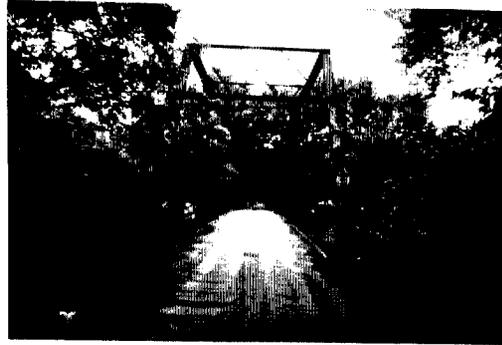
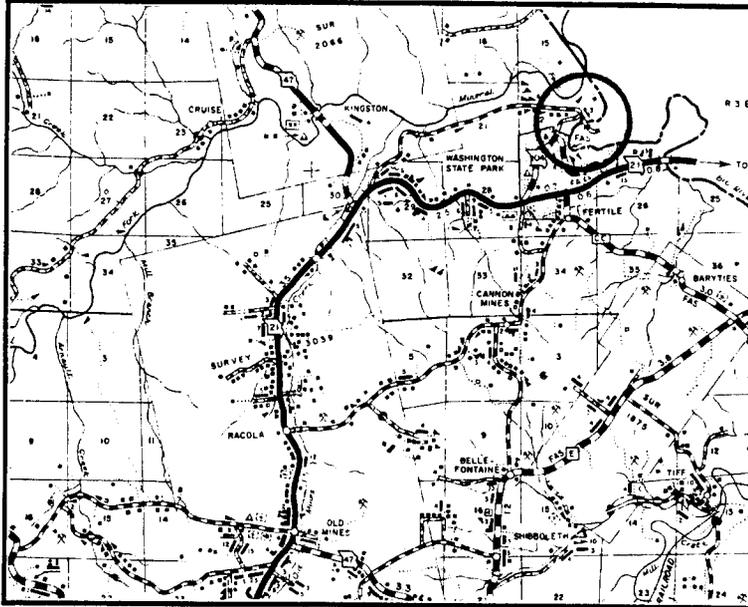
superstructure: wrought iron, 8-panel, pin-connected Pratt through truss with steel stringer approach spans
substructure: concrete-filled steel cylinder piers, with concrete abutments and steel pile bent piers at approach spans
floor/decking: timber deck over steel stringers
other features: upper chord / inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with unslotted turnbuckle; strut: 2 braced angles; floor beam: I-beam, U-bolted to vertical; 2-channel guardrails

After repeated repairs over the years, the timber Lemay Ferry Bridge over the Meramec River between St. Louis and Jefferson counties finally needed replacement by the early 1880s. The two county courts at this time agreed to share the costs of a new structure; after rebuilding the masonry substructure in 1884, the counties in September 1885 adopted plans and specifications prepared by J.T. Garrett, proprietor of the St. Louis Bridge and Iron Company. Competitive bids were solicited, and a contract to fabricate and erect this long-span, pin-connected truss was awarded to St. Louis B&I in November for \$9870.00. By April 1886 the Lemay Ferry Bridge had been completed. The bridge served to carry wagon traffic - and in later years an occasional automobile - until the fall of 1916. That September the Jefferson and St. Louis County Courts decided to replace the aging truss with a new structure. As part of the agreement, Jefferson County took over ownership of the old bridge, which was dismantled by Miller and Borcharding of St. Louis. The county then sold one of its spans to Washington County. Miller and Borcharding subsequently re-erected this span over the Big River at the Kingston Ford on the Jefferson and Washington County line. Here it has carried traffic over the Big River in unaltered condition since early 1917. The Kingston Ford Bridge, and the Lemay Ferry Bridge, has now been in service for more than 100 years.

Large-scale iron truss structures were built at many of Missouri's major river crossings in the 1870s and 1880s, replacing earlier ferry operations. Very few of these big, early trusses have survived to the present, however. The survivors, such as the Lemay Ferry Bridge, are thus technologically and historically significant for their representation of early Missouri overland transportation. A well-preserved truss that has carried traffic at two regionally important crossings, this is one of the more important of the early wagon spans in the state.

NAME(S) OF STRUCTURE

Kingston Ford Bridge (Big River Bridge)

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 013003.7; Jefferson County Court Record 4: page 313 (7 May 1885), page 328 (4 August 1885), page 349 (28 September 1885), page 355 (2 November 1885), page 418 (20 April 1886); Jefferson County Court Record 12: page 411 (20 July 1916), page 443 (23 September 1916), pages 518-20 (19 March 1917), located at Jefferson County Courthouse, Hillsboro MO; Washington County Court Record K: page 321 (1 May 1916), page 333 (4 May 1916), page 349 (30 June 1916), pages 371-72 (12 August 1916), page 396 (14 November 1917), page 403 (18 November 1917); Washington County Court Record L: page 24 (5 April 1917), located at Washington County Courthouse, Potosi MO; field inspection by Clayton Fraser, 6 September 1991.

INVENTORIED BY

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AFFILIATION

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DATE17 January 1992
