

COLE COUNTY

Included: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*COLE01	001000.2	Moniteau Creek Bridge	1-140' pinned Pratt through truss 1916 Kansas City Bridge Company
*COLE02	031000.7	Bridge	(replaced)
*COLE03	031000.8	Koehler's Ford Bridge	1-140' pinned Pratt through truss 1911 Canton Bridge Co., Canton OH
*COLE04	039000.1	Rock House Ford Bridge	(replaced)
*COLE05	047002.8	Robinson's Ferry Bridge	1-170' pinned Parker through truss 1916 Kansas City Bridge Company
*COLE06	069001.6	Wears Creek Bridge	1- 36' concrete filled spandrel arch c1925
COLE07	078003.4	Herbrandt Creek Bridge	(replaced)
COLE08	092002.4	Rising Creek Bridge	(replaced)
*COLE09	165001.9	McCannis Ford Bridge	(replaced)
COLE10	218000.1	Cole Junction Bridge	3- 40' concrete deck girder 1917 Pope Construction Company
COLE11	229001.0	Moreau River Bridge	1-140' riveted Pratt through truss 1934 Drexel Construction Company

EXCLUDED:

Steel stringer

G 302R S 410 S 823 X 35 173001.2 218000.4

Concrete slab

024001.5 039000.7 090000.7 151000.2 176000.8 193000.7

Concrete girder

K 760 Y 310 071002.3 096000.6 113001.0 194000.9 218002.2
228001.9

Concrete box culvert

H 629 J 715 J 717 J 718 K 759R P 93 S 254
S 255 U2180R23 X 483 071001.4 218003.4

Steel pipe culvert

218001.9

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	6	0	0	6
Excluded	15	17	1	0	33
	15	23	1	0	39 structures

Moniteau Creek Bridge

COLE01

GENERAL DATA

structure no.:	001000.2	city/town:	7.2 miles north of Centertown
county:	Cole	feature inters.:	Moniteau Creek
		cadastral grid:	S24, T46N, R14W
		highway route:	County Road 1
		highway distr.:	5
		current owner:	Cole County

STRUCTURAL DATA

superstructure:	steel, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments and wingwalls; concrete-filled steel cylinder piers; laced steel pile bents at east approach		
span number:	1	condition:	fair
span length:	140.0'	alterations:	none
total length:	221.0'	floor/decking :	asphalt-covered timber deck over steel stringers
roadway width:	14.7'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels; portal builder's plate: 1916 / Built by Kansas City Bridge Co. / Kansas City MO. / I.D. Bond Pres. Judge / John Lepage Assoc. Judge / J.R. Taylor Assoc. Judge / Clem Dirokx Clerk / O.L. Moore Dep. Clerk / E.F.C. Harding Engineer; endpost-mounted bridge plate: "Bridge No. 27 Class B

HISTORICAL DATA

erection date:	1915-16
erection cost:	\$5779.00
designer:	Kansas City Bridge Company, Kansas City MO (probable)
fabricator :	Kansas City Bridge Company, Kansas City MO; Inland Steel Company, East Chicago IN
contractor:	Kansas City Bridge Company, Kansas City MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 001000.2; Cole County Court Record, Book Q: page 459 (11 June 1915), page 555 (7 April 1916)- located at the Cole County Courthouse, Jefferson City MO; field inspection by Clayton Fraser, 9 November 1990.

Moniteau Creek Bridge

sign. rating: 46

evaluation: NRHP non-eligible (typically configured, relatively late example of main-stay structural type)

inventoried by: Clayton B. Fraser 15 January 1992

Koehler's Ford Bridge

COLE03

GENERAL DATA

structure no.:	031000.8	city/town:	2.2 miles southwest of St. Martens
county:	Cole	feature inters.:	North Moreau Creek
		cadastral grid:	S8/9, T44N, R13W
		highway route:	County Road 31
		highway distr.:	5
		current owner:	Cole County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Pratt through truss; 9-span steel stringer approach, south end; 2-span steel stringer approach, north end

substructure: concrete abutments; concrete-filled steel cylinder piers under truss; laced, steel pile bent piers under approach spans

span number:	1	condition:	fair
span length:	140.0'	alterations:	none
total length:	469.0'	floor/decking :	asphalt-covered corrugated steel deck over steel stringers
roadway width:	14.7'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 looped square eyebars at the hip); diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice; portal builder's plate: The Canton Bridge Co. / Builders / Canton Ohio

HISTORICAL DATA

erection date: 1910-11

erection cost: \$6775.00 (contract amount)

designer: Canton Bridge Company, Canton OH (probable)

fabricator : Canton Bridge Company, Canton OH;
Cambria Steel Company, Pittsburgh PA

contractor : Canton Bridge Company, Canton OH

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 031000.8; Cole County Court Record, Book P: page 479 (7 October 1910), page 481 (7 November 1910), Cole County Court Record, Book Q: page 12 (10 November 1911), page 25 (4 January 1912), page 84 (12 March 1912) - located at the Cole County Courthouse, Jefferson City MO; **History of Cole, Moniteau, Morgan, Benton, Miller, Maries and Osage Counties, Missouri** (Chicago: Goodspeed Publishing Company, 1889); field inspection by Clayton Fraser, 9 November 1990.

Koehler's Ford Bridge

sign. rating: 54

evaluation: NRHP possibly eligible (well-preserved, well-documented example of mainstay structural type)

inventoried by: Clayton B. Fraser 15 January 1992

Robinson's Ferry Bridge

COLE05

GENERAL DATA

structure no.: 047002.8	city/town: 5.6 miles south of St. Martens
county: Cole	feature inters.: North Moreau Creek
	cadastral grid: S36, T44N, R13W
	highway route: County Road 47
	highway distr.: 5
	current owner: Cole County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Parker through truss, with steel stringer approach span

substructure: concrete abutments at the north end; concrete footers on stone ledge at the south end; concrete pier

span number: 1	condition: fair
span length: 170.0'	alterations: none
total length: 205.0'	floor/decking : asphalt-covered corrugated steel deck over steel stringers
roadway width: 14.5'	

other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: 1 round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 channels; endpost-mounted bridge plate: Class B

HISTORICAL DATA

erection date: 1915-16

erection cost: unknown

designer: Kansas City Bridge and Iron Company, Kansas City MO (probable)

fabricator : Kansas City Bridge and Iron Company, Kansas City MO;
Carnegie Steel Company, Pittsburgh PA;
Cambria Steel Company, Pittsburgh PA

contractor: Kansas City Bridge and Iron Company, Kansas City MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 047002.8; Cole County Court Record Book N: page 623 (February 3 1904); Book Q: page 431 (4 March 1915) - located at the Cole County Courthouse, Jefferson City, MO; field inspection by Clayton Fraser, 9 November 1990.

sign. rating: 53

evaluation: NRHP possibly eligible (well-preserved example of uncommon Pratt truss subtype)

Inventoried by: Clayton B. Fraser 15 January 1992

Wears Creek Bridge

COLE06

GENERAL DATA

structure no.:	069001.6	city/town:	3.0 miles west of Jefferson City
county:	Cole	feature inters.:	Wears Creek
		cadastral grid:	S14, T44N, R12W
		highway route:	County Road 69
		highway distr.:	5
		current owner:	Cole County

STRUCTURAL DATA

superstructure:	concrete filled spandrel arch		
substructure:	concrete abutments and wingwalls		
span number:	1	condition:	fair
span length:	36.0'	alterations:	steel I-beams bolted to arch sidewalls
total length:	56.0'	floor/decking :	concrete deck over earth fill
roadway width:	15.5'	other features:	cantilevered roadway; solid concrete guard-rails with recessed panels

HISTORICAL DATA

erection date: c1925
erection cost: unknown
designer: unknown
fabricator : none
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 069001.6; field inspection by Clayton Fraser, 9 November 1990.

sign. rating: 20
evaluation: NRHP non-eligible (poorly preserved, poorly documented example of early concrete arch construction)

inventoried by: Clayton B. Fraser 15 January 1992

Cole Junction Bridge

COLE10

GENERAL DATA

structure no.:	218000.1	city/town:	Jefferson City
county:	Cole	feature inters.:	Grays Creek
		cadastral grid:	S27, T45N, R12W
		highway route:	County Road 218
		highway distr.:	5
		current owner:	Cole County

STRUCTURAL DATA

superstructure:	concrete deck girder		
substructure:	concrete abutments, wingwalls and piers		
span number:	3	condition:	fair
span length:	40.0'	alterations:	none
total length:	126.0'	floor/decking :	concrete deck
roadway width:	16.2'	other features:	concrete guardrails with turned balusters; bridge plate: BR #6 1917

HISTORICAL DATA

erection date:	1917
erection cost:	unknown
designer:	unknown
fabricator :	none
contractor:	Pope Construction Company
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 218000.1; Cole County Court Record, Book R: pages 46-47 (7 May 1917) - located at Cole County Court-house, Jefferson City MO.
sign. rating:	50
evaluation:	NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)

inventoried by: Clayton B. Fraser 15 January 1992

Moreau River Bridge

COLE11

GENERAL DATA

structure no.:	229001.0	city/town:	4.1 miles south of St. Martens
county:	Cole	feature inters.:	Moreau River
		cadastral grid:	S26, T44N, R13W
		highway route:	County Road 229
		highway distr.:	5
		current owner:	Cole County

STRUCTURAL DATA

superstructure:	steel, 7-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	1	condition:	good
span length:	140.0'	alterations:	unknown
total length:	198.0'	floor/decking :	concrete deck over steel stringers
roadway width:	23.9'	other features:	steel guardrails

HISTORICAL DATA

erection date:	1934
erection cost:	\$17,347.70
designer:	Missouri State Highway Department
fabricator :	unknown
contractor:	Drexel Construction Company
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 229001.0; Primary System Bridge Files - located at MHTD, Jefferson City MO.
sign. rating:	39
evaluation:	NRHP non-eligible (typical example of MSHD truss design of the 1930s)

inventoried by: Clayton B. Fraser 15 January 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Moniteau Creek Bridge
MHTD: 001000.2

COLE01

DATE(S) OF CONSTRUCTION

1915-16

LOCATION

County Road 1 over Moniteau Creek; S24, T46N, R14W
7.2 miles north of Centertown; Cole County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 46)

CONDITION

fair

OWNER

Cole County

span number: 1
span length: 140.0'
total length: 221.0'
roadway wdt.: 14.7'

superstructure: steel, 7-panel, pin-connected Pratt through truss, with steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled steel cylinder piers; laced steel pile bents at east approach
floor/decking: asphalt-covered timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels; portal builder's plate: 1916 / Built by Kansas City Bridge Co. / Kansas City MO. / I.D. Bond Pres. Judge / John Lepage Assoc. Judge / J.R. Taylor Assoc. Judge / Clem Dirokx Clerk / O.L. Moore Dep. Clerk / E.F.C. Harding Engineer; endpost-mounted bridge plate: "Bridge No. 27 Class B

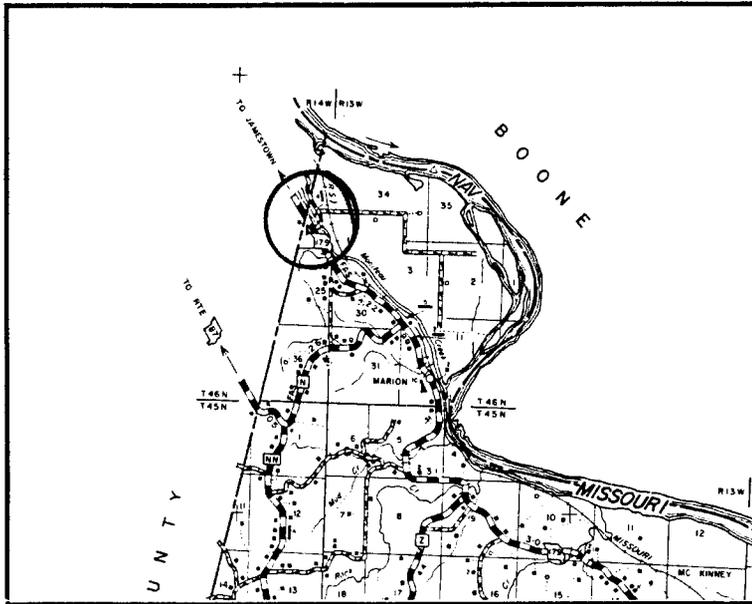
The Moniteau Creek Bridge carries a county road over Moniteau Creek in the uppermost northern region of Cole County. Located some seven miles north of Centertown, this medium-span structure is comprised of a pinned Pratt truss over the stream's channel, with a steel stringer approach span on the west and two steel stringer approach spans on the east. The Moniteau Creek Bridge dates to 1915. Late that spring E.F.C. Harding, the Cole County Engineer, surveyed the site for a permanent span here. The county advertised for competitive bids and on June 11, a contract to fabricate and erect the truss was awarded to the Kansas City Bridge Company for \$5779.00. The firm used components rolled by Inland Steel to fabricate the truss and assembled it over steel cylinder piers later that year. Since its completion, the Moniteau Creek Bridge has carried vehicular traffic, with only maintenance-related repairs.

Like virtually all of Missouri's counties, Cole County followed a definite progression in its bridge construction in the 19th and early 20th centuries, in response to evolving transportation needs and technological development in the bridge industry. The first simple spans, built as the county was undergoing its initial settlement, were rudimentary timber structures. These were cheap and easy to build but lacking in durability and limited in span length. With greater revenues from increased settlement, the county could undertake more ambitious timber/iron combination trusses in the 1860s and 1870s. These, in turn, were superseded in the late 1870s by all-iron spans, made readily available by mass production. Although the county court barely noticed the transition from iron to steel in the

1890s, this evolution marked a watershed that would continue into the 20th century for bridge fabricators and the rolling mills that supplied them. Throughout these later years, the pinned Pratt truss (and its various subtypes) was the bridge of choice for medium- and long-span crossings. In this, it was like most of Missouri's counties. From the 1870s through the 1910s, thousands of pinned Pratt trusses were erected on Missouri's roadways. Although several hundred of these structures remain in use today, relatively few predate the turn of the century. And fewer yet of the 19th century survivors remain structurally intact. With an erection date of 1915-16 and a span length of 140 feet, the Moniteau Creek Bridge falls well within the mainstream of this statewide trend.

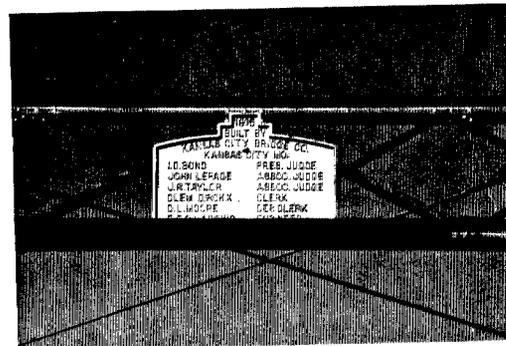
NAME(S) OF STRUCTURE
Moniteau Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 001000.2; Cole County Court Record, Book Q: page 459 (11 June 1915), page 555 (7 April 1916)- located at the Cole County Courthouse, Jefferson City MO; field inspection by Clayton Fraser, 9 November 1990.

INVENTORIED BY
Clayton B. Fraser

AFFILIATION
Fraserdesign, Loveland CO

DATE
15 January 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Koehler's Ford Bridge
MHTD: 031000.8

COLE03

DATE(S) OF CONSTRUCTION

1910-11

LOCATION

County Road 31 over North Moreau Creek; S8/9, T44N, R13W
2.2 miles southwest of St. Martens; Cole County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 54)

CONDITION

fair

OWNER

Cole County

span number: 1
span length: 140.0'
total length: 469.0'
roadway wdt.: 14.7'

superstructure: steel, 8-panel, pin-connected Pratt through truss; 9-span steel stringer approach, south end; 2-span steel stringer approach, north end
substructure: concrete abutments; concrete-filled steel cylinder piers under truss; laced, steel pile bent piers under approach spans
floor/decking: asphalt-covered corrugated steel deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 looped square eyebars at the hip); diagonal: 2 looped rectangular eyebars; lateral bracing: round rod with threaded ends; strut: 2 angles; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice; portal builder's plate: **The Canton Bridge Co. / Builders / Canton Ohio**

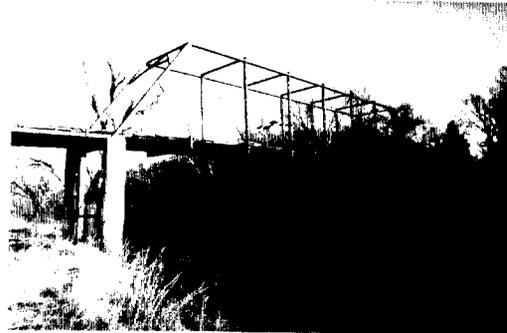
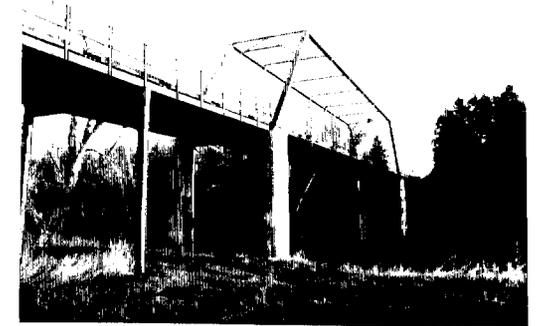
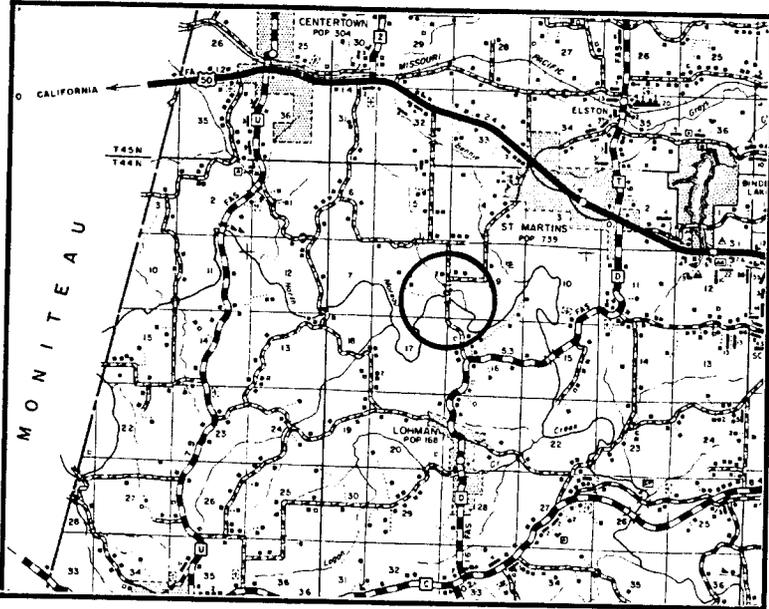
In October 1910 the Cole County Court instructed county surveyor E.F.C. Harding to visit a stream crossing of a county road in northwestern Cole County. Known locally as Koehler's Ford, this crossing of North Moreau Creek was located southwest of St. Martens. The surveyor prepared cost estimates for a "steel highway bridge across North Moreau Creek at or near Koehler's Ford" that would "conform to state highway engineers Class B bridge" specifications. Later that month the county solicited competitive bids for the bridge's fabrication and erection, awarding the contract to the Canton Bridge Company of Canton, Ohio, for \$6775.00. As built (and probably designed) by Canton, the multiple-span structure consisted of a pin-connected Pratt truss over the river's channel, with an extensive steel stringer trestle over the flood plain to the south. Concrete-filled cylinder piers supported the truss; steel pile bents with laced lateral bracing were located under the approach spans. Canton used steel components rolled in the Cambria mills of Pittsburgh to fabricate the truss, substructure and approaches. Completed early in 1911, the Koehler's Ford Bridge has functioned in place since, in essentially unaltered condition.

As one of America's most prolific bridge fabricators, the Canton Bridge Company maintained an extensive catalogue of truss types, ranging from the exotic to the commonplace. Canton, like most of the region's bridge builders of the time, relied heavily on pin-connected Pratt truss variants for its standard truss types. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design was characterized by upper chords and vertical members acting in compression and lower chords and diagonals that acted in tension. Its parallel chords and equal panel lengths resulted in standardized sizes for the verticals, diagonals and chord members, making fabrication and assembly relatively

NAME(S) OF STRUCTURE

Koehler's Ford Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 031000.8; Cole County Court Record, Book P: page 479 (7 October 1910), page 481 (7 November 1910), Cole County Court Record, Book Q: page 12 (10 November 1911), page 25 (4 January 1912), page 84 (12 March 1912) - located at the Cole County Courthouse, Jefferson City MO; **History of Cole, Moniteau, Morgan, Benton, Miller, Maries and Osage Counties, Missouri** (Chicago: Goodspeed Publishing Company, 1889); field inspection by Clayton Fraser, 9 November 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

15 January 1992

easy. In the highly competitive bridge manufacturing industry, in which efficiency equated with profit, Pratt trusses received almost universal use. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty feet in length," noted bridge engineer J.A.L. Waddell wrote in 1916. "Its advantages are simplicity, economy of metal, and suitability for connecting to the floor and lateral systems." Virtually all of the major regional fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties in the late 19th and early 20th centuries. The Koehler's Ford Bridge is distinguished among these for its excellent state of preservation: with its truss, substructure and extensive approach spans in place, it is an important transportation-related resource.

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Robinson's Ferry Bridge
MHTD: 047002.8

COLE05

DATE(S) OF CONSTRUCTION

1915-16

LOCATION

County Road 47 over North Moreau Creek; S36, T44N, R13W
5.6 miles south of St. Martens; Cole County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Cole County

span number: 1
span length: 170.0'
total length: 205.0'
roadway wdt.: 14.5'

superstructure: steel, 8-panel, pin-connected Parker through truss, with steel stringer approach span
substructure: concrete abutments at the north end; concrete footers on stone ledge at the south end; concrete pier
floor/decking: asphalt-covered corrugated steel deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: 1 round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to verticals; guardrail: 2 channels; endpost-mounted bridge plate: **Class B**

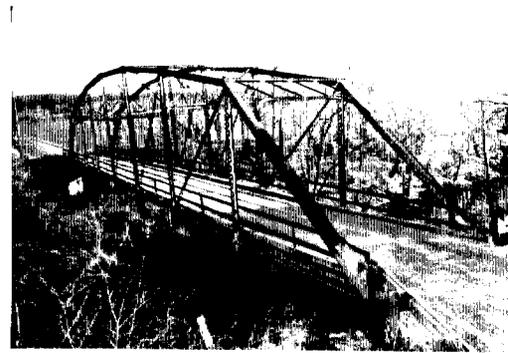
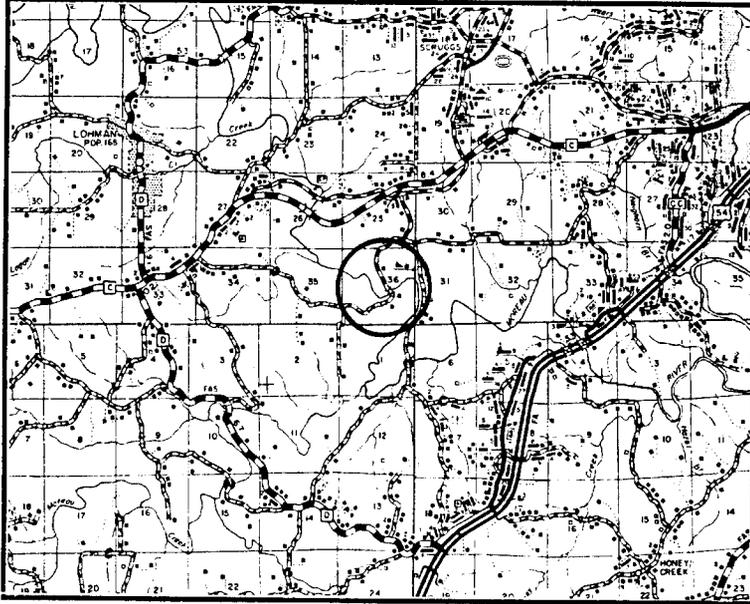
Located south of St. Martens in central Cole County, this medium-span truss carries a gravel-surfaced road over North Moreau Creek. The bridge features a pin-connected Parker through truss, supported by a concrete substructure and approached by a single steel stringer span on the north. Built in 1916, the structure has its origins early in the 20th century. Early in 1904 a group of citizens from Moreau Township petitioned the Cole County Court for a permanent bridge over North Moreau Creek to replace the ferryboat operation at Robinson's Ferry. The petitioners offered a \$2000.00 subscription to help defray the cost of the bridge's construction, but the county delayed action on the bridge for eleven years. In 1915 the judges finally advertised for competitive bids to build the bridge. When proposals were received, the court accepted the bid of the Kansas City Bridge and Iron Company to fabricate and erect the truss for \$7600.00. Completed in 1916, the Robinson's Ferry Bridge has since carried vehicular traffic, with the strengthening of a strut as the only modification of note.

With their relatively long spans, light structural members and archaic detailing, pin-connected Parker through trusses have proved particularly vulnerable in Missouri's bridge replacement program. As a result, of the hundreds that once carried vehicular traffic through the state, only some 35 have been identified as extant by the historic bridge inventory. These range in span length from 110 feet to 200 feet and from erection date from 1900 to 1932. Typical of Missouri's trusses, they vary widely in their retention of physical integrity as well. The Robinson's Ferry Bridge, with its 170-foot span and 1915-16 construction date, falls within the milieu of this trend. It is noteworthy for its excellent state of preservation.

NAME(S) OF STRUCTURE

Robinson's Ferry Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 047002.8; Cole County Court Record Book N: page 623 (February 3 1904); Book Q: page 431 (4 March 1915) - located at the Cole County Courthouse, Jefferson City, MO; field inspection by Clayton Fraser, 9 November 1990.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

15 January 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Cole Junction Bridge
MHTD: 218000.1

COLE10

DATE(S) OF CONSTRUCTION

1917

LOCATION

County Road 218 over Grays Creek; S27, T45N, R12W
Jefferson City; Cole County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 50)

CONDITION

fair

OWNER

Cole County

span number: 3
span length: 40.0'
total length: 126.0'
roadway wdt.: 16.2'

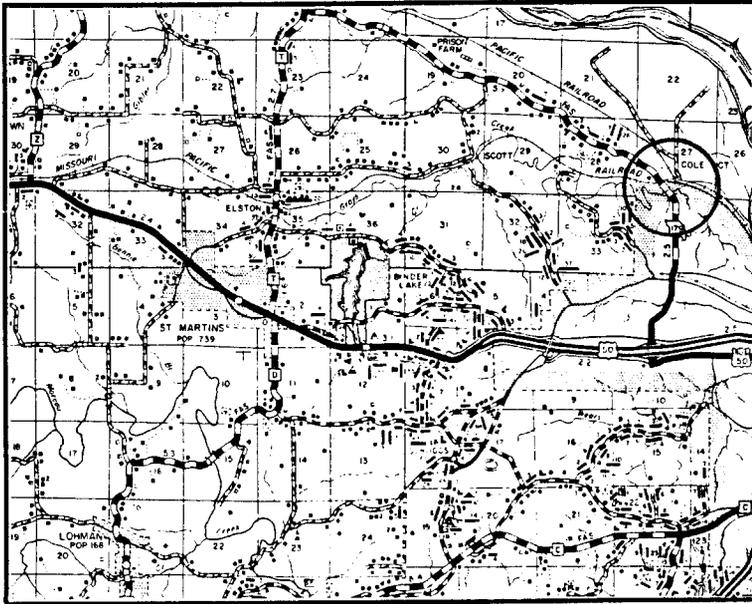
superstructure: concrete deck girder
substructure: concrete abutments, wingwalls and piers
floor/decking: concrete deck
other features: concrete guardrails with turned balusters; bridge plate: BR #6 1917

Located at Cole Junction, near the northern outskirts of Jefferson City, this bridge spans Grays Creek. The structure consists of three concrete deck girder spans, supported by concrete piers and abutments. Molded concrete balusters in the guardrails form the bridge's most noteworthy architectural expression. The Cole Junction Bridge dates to 1917. In April of that year the Cole County Court contracted with Joseph Pope, owner of the Pope Construction Company, to build a "Class A concrete bridge" at this point, a concrete span over Grays Creek at Buckner Ford and another smaller structure over Herbrandt Branch on the Thomas - Jefferson City road. (Both of the latter structures have recently been demolished.) The cost of the Cole Junction Bridge was \$6160.00. Completed later that year, it has functioned in place without substantial alteration.

Unlike many Midwestern states, Missouri did not employ reinforced concrete extensively for construction of vehicular bridge superstructures in the 1910s. The various counties and, to a lesser extent, the state highway department continued to prefer steel for bridge superstructures well after concrete had received widespread acceptance elsewhere. This, combined with subsequent attrition, has resulted in a relatively small number of concrete bridges that exist today from this formative period. The Cole Junction Bridge is distinguished among these as one of the earliest concrete deck girders found in the state. Modestly scaled and simply detailed, it is noteworthy as an early, well-preserved example of concrete bridge construction in Missouri.

NAME(S) OF STRUCTURE
Cole Junction Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 218000.1; Cole County Court Record, Book R: pages 46-47 (7 May 1917) - located at Cole County Courthouse, Jefferson City MO.

INVENTORIED BY
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DATE
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