

SHELBY COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
*SHEL01	017001.3	McCully Bridge	1-150' pinned Pratt through truss 1891
*SHEL02	020002.2	Black Creek Bridge	1- 45' pinned Pratt bedstead 1907 Pan-American Bridge Company
*SHEL03	126000.1	South Fabius River Bridge	1-170' pinned Camelback through truss 1899
SHEL04	127000.2	Jones Branch Bridge	1- 36' riveted lattice bedstead 1909 Michelmann Steel Constr. Co.
SHEL05	171001.8	Kirby Bridge	1- 60' pinned Pratt bedstead c1910
SHEL06	176001.7	Tannehill Bridge	1- 30' riveted lattice bedstead 1907 G.T. Meredith
SHEL07	231001.0	Clear Creek Bridge	(replaced)
*SHEL08	290000.3	Walkersville Bridge	1-100' riveted Pratt pony truss c1930
*SHEL09	300000.3	Salt River Bridge	1-110' riveted Pratt pony truss c1930
SHEL10	307000.5	Turney Bridge	1- 28' riveted lattice bedstead 1907 Pan-American Bridge Company
SHEL11	310000.2	Hammett Bridge	(replaced)
*SHEL12	320001.4	Conboy Bridge	1-100' pinned Pratt through truss 1910 Pan American Bridge Company
*SHEL13	321001.2	Gillis Bridge	1- 80' pinned Pratt bedstead 1908 Pan American Bridge Company
*SHEL14	333000.5	Jordan Bridge	1- 80' pinned Pratt bedstead 1907 Michelmann Steel Constr. Co.
SHEL15	379001.5	Maupin Bridge	1- 36' riveted lattice bedstead 1911 Decatur Bridge Company
SHEL16	392000.9	Kidwell Bridge	(replaced)

EXCLUDED:

Pratt pony truss						
112000.4	207001.1	214000.3	215000.4	339000.4	340000.3	349000.3
Warren pony truss						
020002.2	034001.1	134000.8	187000.7	224000.4	383000.8	399001.2
Lattice bedstead						
044000.6	072000.6	084001.6	202002.2	275001.1	277001.5	304000.8
324000.7	332001.6					

SHELBY COUNTY

EXCLUDED (cont.):

Steel stringer

H 590R	J 768R	L 167	P 40	S 427	S 711	T 391
T 601	X 125	X 211	X 496	033000.1	076000.4	090000.6
100000.9	134000.8	134001.1	173000.7	194000.1	207001.3	217000.5
221000.3	246000.5	253001.0	276001.2	341001.3	367000.1	370000.1
371000.1	375001.4	379003.3	387001.1	396000.8	397000.1	500000.7

Concrete girder

G 990	H 588	H 589
-------	-------	-------

Concrete slab

043000.3

Concrete box culvert

H 683	K 192	K 193	K 195	K 196	K 280	S 227
S 228	T 215	T 229	X 274			

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	0	13	0	0	13
Excluded	25	48	0	0	73
	<hr/>				
	25	61	0	0	86 structures

McCully Bridge

SHEL01

GENERAL DATA

structure no.: 017001.3 city/town: 5.1 miles northwest of Leonard
county: Shelby feature inters.: Salt River
cadastral grid: S8/17, T59N, R12W
highway route: County Road 17
highway distr.: 3
current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Pratt through truss
substructure: steel pile bent abutments with timber back- and wingwalls

span number: 1 condition: fair
span length: 150.0' alterations: truss moved from outside the county, 1901
total length: 151.0' floor/decking : timber deck over steel stringers
roadway width: 13.6' other features: upper chord and inclined end post: 2 channels
with cover and batten plates; lower chord: 2
punched rectangular eyebars; vertical: 2 chan-
nels with lacing (2 looped square eyebars at
the hip); diagonal: 2 punched rectangular
eyebars; counter: round eyerod with turn-
buckle; lateral bracing: round eyerod with
turnbuckle - upper, round rod with threaded
ends - lower; portal strut: latticed angles;
floor beam: plate girder, U-bolted to vertical;
guardrail: 2 channels; endpost-mounted buil-
der's plate [broken] 1891...

HISTORICAL DATA

erection date: 1891
erection cost: \$994.00 (cost of re-erection in 1901)
designer: unknown
fabricator : unknown
contractor: American Bridge Company (1901 re-erection)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 017001.3; Shelby County Court Record, Book 15: page 531 (6 August 1901), page 543 (3 September 1901), page 554 (8 October 1901); Shelby County Court Record, Book 16: page 82 (6 August 1902), page 213 (6 May 1903) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

sign. rating: 31
evaluation: NRHP non-eligible (early, well-preserved example of mainstay structural type, but lacking in documentation and moved from original location)

Inventoried by: Clayton B. Fraser 3 February 1992

Black Creek Bridge

SHEL02

GENERAL DATA

structure no.: 020002.2	city/town: 1.3 miles northwest of Leonard
county: Shelby	feature inters.: Black Creek
	cadastral grid: S13/24, T59N, R12W
	highway route: County Road 20
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead	
substructure: bedstead legs with timber back- and wingwalls	
span number: 1	condition: fair
span length: 45.0'	alterations: none
total length: 46.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: steel lattice guardrails

HISTORICAL DATA

erection date: 1907
erection cost: \$795.00
designer: unknown
fabricator : unknown
contractor : Pan-American Bridge Company
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 020002.2; Shelby County Court Record, Book 17: page 321 (2 July 1907) - located at Shelby County Courthouse Shelbyville MO.
sign. rating: 39
evaluation: NRHP non-eligible (typical example of common Pratt truss sub-type)

inventoried by: Clayton B. Fraser 3 February 1992

South Fabius River Bridge

SHEL03

GENERAL DATA

structure no.: 126000.1	city/town: 8.3 miles northeast of Bethel
county: Shelby	feature inters.: South Fabius River
	cadastral grid: S4/33, T59/60N, R9W
	highway route: County Road 126
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 9-panel, pin-connected Camelback through truss
substructure: concrete abutments

span number: 1	condition: fair
span length: 170.0'	alterations: truss moved to this location
total length: 172.0'	floor/decking : timber deck over steel stringers
roadway width: 13.5'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 square eye-bars with unslotted turnbuckles; lateral bracing: round eyebar with unslotted turnbuckle (upper), round rod with threaded ends (lower); strut: 2 braced angles; portal strut: steel lattice with decorative cresting and curved portal knee braces; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

HISTORICAL DATA

erection date: 1899
erection cost: unknown
designer: unknown
fabricator : Carnegie Steel Company, Pittsburgh PA
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 126000.1; Illustrated Historical Atlas of Shelby County, Missouri (Philadelphia: Edwards Bros, 1878) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

sign. rating: 48
evaluation: NRHP determined non-eligible (long-span example of uncommon Pratt truss subtype, moved from outside the county)

inventoried by: Clayton B. Fraser 3 February 1992

Jones Branch Bridge

SHEL04

GENERAL DATA

structure no.: 127000.2	city/town: 8.4 miles northeast of Bethel
county: Shelby	feature inters.: Jones Branch
	cadastral grid: S10, T59N, R9W
	highway route: County Road 127
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected, lattice bedstead, with steel stringer approach span	
substructure: timber abutments, wingwalls and piers	
span number: 1	condition: fair
span length: 36.0'	alterations: unknown
total length: 51.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: no guardrails

HISTORICAL DATA

erection date: 1909	
erection cost: unknown	
designer: unknown	
fabricator : unknown	
contractor: Michelmann Steel Construction Company, Quincy IL	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 127000.2; Shelby County Court Record, Book 18: page 38 (7 June 1909); Illustrated Historical Atlas of Shelby County, Missouri (Philadelphia: Edwards Bros, 1878) - located at Shelby County Courthouse, Shelbyville MO.	
sign. rating: 35	
evaluation: NRHP non-eligible (short-span example of relatively common Pratt truss sub-type, exhibiting below-average physical integrity)	

inventoried by: Clayton B. Fraser 3 February 1992

Kirby Bridge

SHEL05

GENERAL DATA

structure no.: 171001.8	city/town: 5.5 miles northwest of Shelbyville
county: Shelby	feature inters.: Black Creek
	cadastral grid: S10, T58N, R11W
	highway route: County Road
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt truss-leg bedstead	
substructure: steel pile bent piers	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 60.0'	floor/decking : timber deck over steel stringers
roadway width: 11.7'	other features: steel lattice guardrails

HISTORICAL DATA

erection date: c1910
erection cost: unknown
designer: unknown
fabricator : unknown
contractor : unknown
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 171001.8.
sign. rating: 28
evaluation: NRHP non-eligible (undocumented, typically configured example of relatively common Pratt truss sub-type; marginal integrity)

inventoried by: Clayton B. Fraser 3 February 1992

Tannehill Bridge

SHEL06

GENERAL DATA

structure no.: 176001.7	city/town: 1.7 miles west of Shelbyville
county: Shelby	feature inters.: unnamed stream
	cadastral grid: S24/25, T58N, R11W
	highway route: County Road 176
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected, lattice bedstead	
substructure: steel pile bent piers	
span number: 1	condition: fair
span length: 30.0'	alterations: unknown
total length: 30.0'	floor/decking : concrete deck over steel stringers
roadway width: 11.8'	other features: no guardrails

HISTORICAL DATA

erection date: 1907
erection cost: unknown
designer: unknown
fabricator : Pan-American Bridge Company
contractor: G.T. Meredith
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 176001.7; Shelby County Court Record, Book 17: pages 256-57 (5 April 1907) - located at Shelby County Court-house, Shelbyville MO.
sign. rating: 32
evaluation: NRHP determined non-eligible (relatively common small-scale truss type, with marginal integrity)

inventoried by: Clayton B. Fraser 3 February 1992

Walkersville Bridge

SHEL08

GENERAL DATA

structure no.: 290000.3	city/town: 3.8 miles north of Shelbina
county: Shelby	feature inters.: Salt River
	cadastral grid: S18, T57N, R10W
	highway route: County Road 290
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt pony truss
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 100.0'	alterations: none
total length: 101.0'	floor/decking : concrete deck over steel stringers
roadway width: 11.6'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 4 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 4 angles with batten plates; lateral bracing: 1 angle; floor beam: I-beam, field-bolted to vertical; guard-rail: steel lattice

HISTORICAL DATA

erection date: c1930
erection cost: unknown
designer: unknown
fabricator : Carnegie Steel Company, Pittsburgh PA
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 290000.3; Shelby County Court Record, Book 15: page 213 (5 September 1899), page 262 (5 December 1899) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 September 1991.

sign. rating: 28
evaluation: NRHP non-eligible (longer than average, but typically configured, poorly documented example of a common structural type)

inventoried by: Clayton B. Fraser 3 February 1992

Salt River Bridge

SHEL09

GENERAL DATA

structure no.:	300000.3	city/town:	5.0 miles southeast of Shelbyville
county:	Shelby	feature inters.:	Salt River
		cadastral grid:	S15, T57N, R10W
		highway route:	County Road 300
		highway distr.:	3
		current owner:	Shelby County

STRUCTURAL DATA

superstructure: steel, 6-panel, rigid-connected Pratt pony truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	good
span length:	110.0'	alterations:	none
total length:	111.0'	floor/decking :	concrete deck over steel stringers
roadway width:	11.6'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: wide flange; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 1 angle; floor beam: I-beam, field-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: c1930
erection cost: unknown
designer: unknown
fabricator : Illinois Steel Company, Chicago IL
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 300000.3.

sign. rating: 33
evaluation: NRHP non-eligible (typically configured, undocumented, long-span example of a common structural type)

inventoried by: Clayton B. Fraser 3 February 1992

Turney Bridge

SHEL10

GENERAL DATA

structure no.: 307000.5	city/town: 3.6 miles east of Shelbina
county: Shelby	feature inters.: unnamed stream
	cadastral grid: S35/36, T57N, R10W
	highway route: County Road 307
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected lattice bedstead	
substructure: concrete abutments and wingwalls	
span number: 1	condition: fair
span length: 28.0'	alterations: unknown
total length: 29.0'	floor/decking : timber deck over steel stringers
roadway width: 10.9'	other features: no guardrails

HISTORICAL DATA

erection date: 1907	
erection cost: \$285.00	
designer: unknown	
fabricator : unknown	
contractor: Pan-American Bridge Company	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 307000.5; Shelby County Court Record, Book 17: page 307 (4 June 1907), page 321 (2 July 1907) - located at Shelby County Courthouse, Shelbyville MO.
sign. rating: 32	
evaluation:	NRHP non-eligible (relatively common small-scale truss, with marginal integrity and incomplete documentation)

inventoried by: Clayton B. Fraser 3 February 1992

Conboy Bridge

SHEL12

GENERAL DATA

structure no.: 320001.4	city/town: 4.7 miles northeast of Shelbina
county: Shelby	feature inters.: Salt River
	cadastral grid: S24, T57N, R10W
	highway route: County Road 320
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss; 2-panel lattice bedstead approach span at the north end; 1 steel stringer approach span at the south end

substructure: concrete abutments and wingwalls; concrete-filled steel cylinder piers

span number: 1	condition: fair
span length: 100.0'	alterations: none
total length: 158.0'	floor/decking : timber deck over steel stringers
roadway width: 11.3'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; portal strut: latticed angles with curved knee braces; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

HISTORICAL DATA

erection date: 1910
erection cost: \$2795.00
designer: unknown
fabricator : unknown
contractor: Pan-American Bridge Company

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 320001.4; Shelby County Court Record, Book 18: page 212 (4 May 1910), page 240 (6 June 1910), page 340 (3 January 1911); Illustrated Historical Atlas of Shelby County, Missouri (Philadelphia: Edwards Bros, 1878) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

sign. rating: 37
evaluation: NRHP non-eligible (typically configured example of common structural type)

inventoried by: Clayton B. Fraser 3 February 1992

Gillis Bridge

SHEL13

GENERAL DATA

structure no.: 321001.2	city/town: 4.5 miles southeast of Shelbyville
county: Shelby	feature inters.: Black Creek
	cadastral grid: S1/2, T57N, R10W
	highway route: County Road 321
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt truss-leg bedstead
substructure: steel truss legs with timber backwalls

span number: 1	condition: good
span length: 80.0'	alterations: none
total length: 81.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: upper chord and end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

HISTORICAL DATA

erection date: 1908
erection cost: \$615.00
designer: unknown
fabricator : Lackawanna Steel Company, Pittsburgh PA
contractor : Michelmann Steel Construction Company, Quincy IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 321001.2; Shelby County Court Record, Book 17: page 378 (3 December 1907), page 476 (2 June 1908), page 558 (4 January 1909) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

sign. rating: 48
evaluation: NRHP possibly eligible (one of Missouri's longest-span example of pinned bedstead construction)

inventoried by: Clayton B. Fraser 3 February 1992

Jordan Bridge

SHEL14

GENERAL DATA

structure no.: 333000.5	city/town: 6.7 miles northeast of Shelbina
county: Shelby	feature inters.: Black Creek
	cadastral grid: S7, T57N, R9W
	highway route: County Road 333
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt truss-leg bedstead
substructure: steel pile bent piers with timber back- and wingwalls

span number: 1	condition: fair
span length: 80.0'	alterations: truss legs replaced with steel pile bent piers
total length: 81.0'	floor/decking : timber deck over steel stringers
roadway width: 12.0'	other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

HISTORICAL DATA

erection date: 1907
erection cost: \$650.00
designer: unknown
fabricator : Lackawanna Steel Company, Pittsburgh PA
contractor : Michelmann Steel Construction Company, Quincy IL

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 333000.5; Shelby County Court Record, Book 16: page 385 (4 May 1904); Shelby County Court Record, Book 17: page 307 (4 June 1907) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

sign. rating: 44
evaluation: NRHP non-eligible (one of Missouri's longest-span example of pinned bedstead construction)

inventoried by: Clayton B. Fraser 3 February 1992

Maupin Bridge

SHEL15

GENERAL DATA

structure no.: 379001.5	city/town: 6.8 miles south of Clarence
county: Shelby	feature inters.: Otter Creek
	cadastral grid: S15/22, T56N, R12W
	highway route: County Road
	highway distr.: 3
	current owner: Shelby County

STRUCTURAL DATA

superstructure: steel, 3-panel, rigid-connected, lattice bedstead	
substructure: unknown	
span number: 1	condition: fair
span length: 36.0'	alterations: unknown
total length: 37.0'	floor/decking : timber deck over steel stringers
roadway width: 12.0'	other features: unknown

HISTORICAL DATA

erection date: 1911	
erection cost: \$1414.00 (for three bridges)	
designer: unknown	
fabricator : unknown	
contractor : Decatur Bridge Company, Decatur IL	
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 379001.5; Shelby County Court Record, Book 18: page 38 (7 June 1909), page 453 (3 July 1911) - located at Shelby County Courthouse, Shelbyville MO.
sign. rating: 33	
evaluation:	NRHP non-eligible (undocumented, undistinguished, small-scale structure, lacking in technological significance)

inventoried by: Clayton B. Fraser 3 February 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

South Fabius River Bridge
MHTD: 126000.1

SHEL03

DATE(S) OF CONSTRUCTION

1899

LOCATION

County Road 126 over South Fabius River; S4/33, T59/60N, R9W
8.3 miles northeast of Bethel; Shelby County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP determined non-eligible (score: 48)

CONDITION

fair

OWNER

Shelby County

span number: 1
span length: 170.0'
total length: 172.0'
roadway wdt.: 13.5'

superstructure: steel, 9-panel, pin-connected Camelback through truss
substructure: concrete abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: 2 square eyebars with unslotted turnbuckles; lateral bracing: round eyebar with unslotted turnbuckle (upper), round rod with threaded ends (lower); strut: 2 braced angles; portal strut: steel lattice with decorative cresting and curved portal knee braces; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

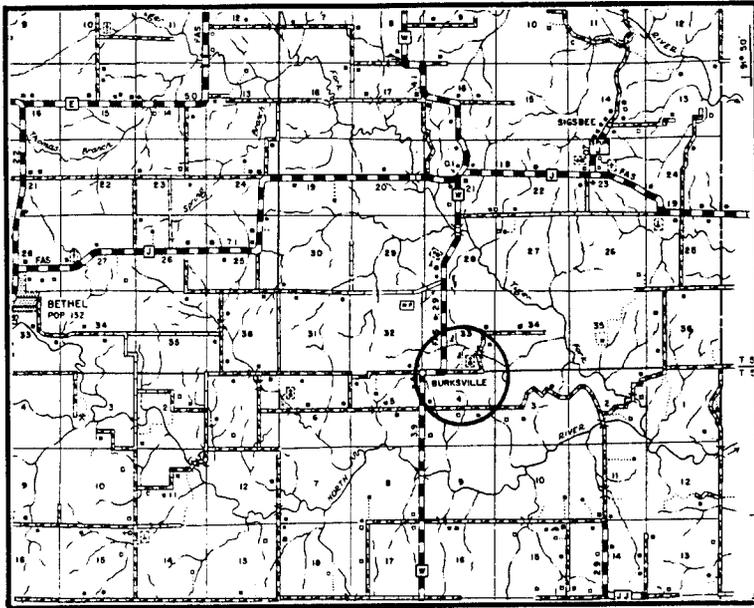
The South Fabius River Bridge carries a gravel-surfaced county road about eight miles northeast of Bethel on the Lewis County line. A bridge plate (now removed but visible in historic photographs), denotes that the structure was built in 1899. An early, long-span, pin-connected Camelback through truss, the bridge was evidently moved to this location in the early part of the century. For long-span crossings, especially over the Salt and Fabius Rivers, Shelby County officials often eschewed new construction, opting instead to purchase existing trusses and have them re-erected. Such was the case with the South Fabius River Bridge, and the history of its original construction is thus unknown. The bridge, nonetheless, appears structurally intact, and has functioned in its present location between Shelby and Lewis Counties for well over fifty years.

Pin-connected Pratt trusses were built by the thousands throughout Missouri in the late 19th and early 20th centuries. Camelback trusses were much less common, and relatively few remain in place today. Built in 1899, the South Fabius River Bridge is the earliest documented of only nine pinned Camelback through trusses known to still exist statewide. The bridge's lack of documentation is mitigated by its early erection date, and the rarity of its type. Having long since acquired a sense of time and place at its current location, South Fabius River Bridge is a well-preserved example of a now rare structural type - the pinned Camelback through truss.

NAME(S) OF STRUCTURE

South Fabius River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 126000.1; Illustrated Historical Atlas of Shelby County, Missouri (Philadelphia: Edwards Bros, 1878) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

3 February 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Conboy Bridge
MHTD: 320001.4

SHEL12

DATE(S) OF CONSTRUCTION

1910

LOCATION

County Road 320 over Salt River; S24, T57N, R10W
4.7 miles northeast of Shelbina; Shelby County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 37)

CONDITION

fair

OWNER

Shelby County

span number: 1

span length: 100.0'

total length: 158.0'

roadway wdt.: 11.3'

superstructure: steel, 6-panel, pin-connected Pratt through truss; 2-panel lattice bedstead approach span at the north end; 1 steel stringer approach span at the south end
substructure: concrete abutments and wingwalls; concrete-filled steel cylinder piers
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing; diagonal: 2 looped rectangular eyebars; counter: square eyebar with turnbuckle; lateral bracing: round rod with threaded ends; strut: 2 angles with continuous plate; portal strut: latticed angles with curved knee braces; floor beam: I-beam, field-bolted to vertical; guardrail: steel lattice

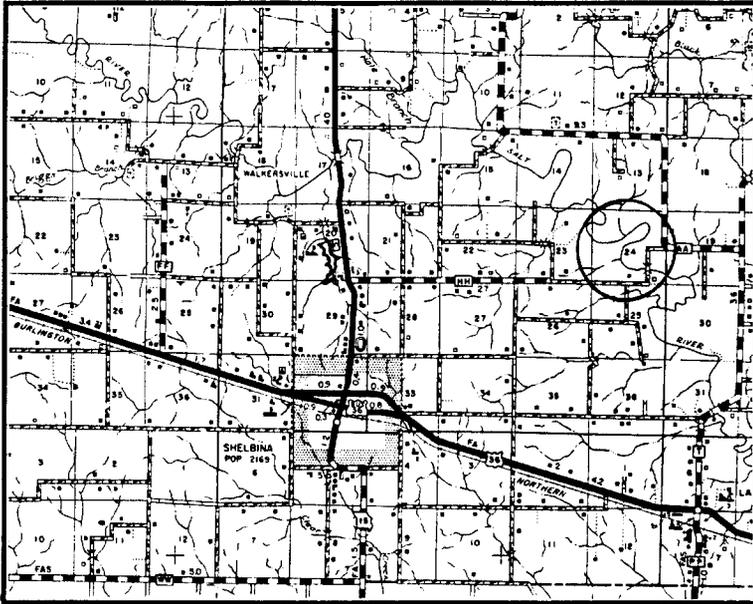
The Conboy Bridge carries a secondary county road over the Salt River slightly under five miles northeast of Shelbina, in southeastern Shelby County. A pin-connected Pratt through truss, the structure is approached on its north end by a latticed pony truss. Support is provided by concrete abutments and concrete-filled steel cylinder piers. Efforts to build the crossing began in the spring of 1910. At a meeting of the Shelby County Court, held on May 4, 1910, county highway engineer, W.W. Mitchel reported that he had estimated the costs of four steel bridges. The largest of these was the Conboy Bridge - named for adjacent landowner Thomas Conboy - over the Salt River. A month later, on June 6th, the Pan American Bridge Company was awarded a \$2795.00 contract for the structure's erection. Based in New Castle, Indiana, Pan American was a prominent bridge building firm, operating throughout the midwest. Another, smaller, Pan American Bridge Company was also active in northeast Missouri, however. Based in nearby Moberly, this firm often competed with the larger Indiana firm of the same name. It is unclear which of the two companies built this bridge. The Conboy Bridge was completed according to plan, and subsequently served to carry local traffic in a rural location. Displaying strong historical integrity, the structure's appears much the same as when originally built.

In Missouri, the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. As a result, thousands of Pratts were built across the state, and today Pratts constitute the most populous group of through trusses. Though it retains a relatively high degree of physical integrity, the Conboy Bridge is unremarkable in its design, dimensions and detailing.

NAME(S) OF STRUCTURE

Conboy Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 320001.4; Shelby County Court Record, Book 18: page 212 (4 May 1910), page 240 (6 June 1910), page 340 (3 January 1911); Illustrated Historical Atlas of Shelby County, Missouri (Philadelphia: Edwards Bros, 1878) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

3 February 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Gillis Bridge
MHTD: 321001.2

SHEL13

DATE(S) OF CONSTRUCTION

1908

LOCATION

County Road 321 over Black Creek; S1/2, T57N, R10W
4.5 miles southeast of Shelbyville; Shelby County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 48)

CONDITION

good

OWNER

Shelby County

<p>span number: 1 span length: 80.0' total length: 81.0' roadway wdt.: 11.8'</p>	<p>superstructure: steel, 5-panel, pin-connected Pratt truss-leg bedstead substructure: steel truss legs with timber backwalls floor/decking: timber deck over steel stringers other features: upper chord and end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels</p>
--	--

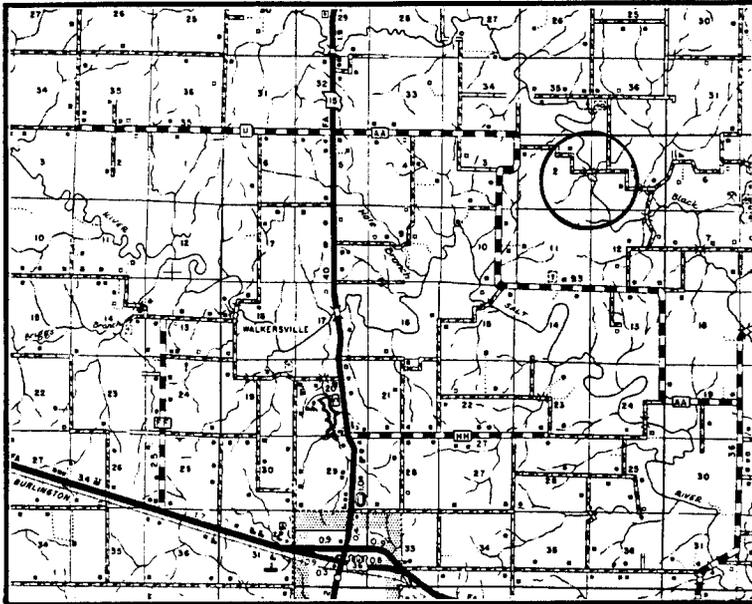
The Gillis Bridge carries a secondary county road over the Black River some four miles southeast of Shelbyville, in southeastern Shelby County. A pin-connected Pratt truss-leg bedstead, the structure is supported by its original bedstead legs. Its 80-foot span rates among the state's longest on a bedstead design. Efforts to bridge the crossing began in late 1907. At a meeting of the Shelby County Court, held on December 3, 1907, county highway engineer W.W. Mitchel reported on the necessity of two bridges. One of these was a short-span, steel stringer, while the other was this 80-foot bedstead, known as the Gillis Bridge. Plans to build the Gillis Bridge were put on hold until the following spring. On June 2, 1908, the Michelmann Steel Construction Company of Quincy, Illinois, submitted a low bid of \$615.00 for the structure's erection. The county court initially tabled the offer, but at a later meeting decided to accept it. On January 4, 1909, Shelby County issued a \$615.00 warrant to Michelmann for completion of the Gillis Bridge. Having served to carry local traffic in a rural location, the structure has suffered virtually no loss of physical integrity over the years.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the later 1890s and early 1900s. Hundreds remain in place today - in fact, Missouri has probably more bedsteads than any other state. The Gillis Bridge is distinguished as a well-preserved, long span example of this statewide bridge construction trend. In fact, its 80-foot length is unexcelled by any other bedstead in the state.

NAME(S) OF STRUCTURE

Gillis Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 321001.2; Shelby County Court Record, Book 17: page 378 (3 December 1907), page 476 (2 June 1908), page 558 (4 January 1909) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

3 February 1992

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Jordan Bridge
MHTD: 333000.5

SHEL14

DATE(S) OF CONSTRUCTION

1907

LOCATION

County Road 333 over Black Creek; S7, T57N, R9W
6.7 miles northeast of Shelbina; Shelby County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 44)

CONDITION

fair

OWNER

Shelby County

span number: 1
span length: 80.0'
total length: 81.0'
roadway wdt.: 12.0'

superstructure: steel, 5-panel, pin-connected Pratt truss-leg bedstead
substructure: steel pile bent piers with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: upper chord and upright end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars, or 2 angles with batten plates; vertical: 4 angles with lacing; diagonal: 2 square eyebars; counter: square eyerod with turnbuckle; lateral bracing: round rod with threaded ends; floor beam: I-beam, field-bolted to vertical; guardrail: 2 channels

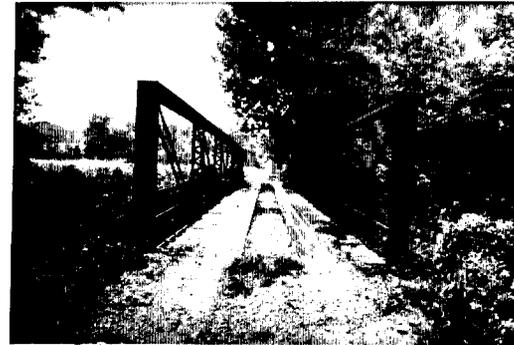
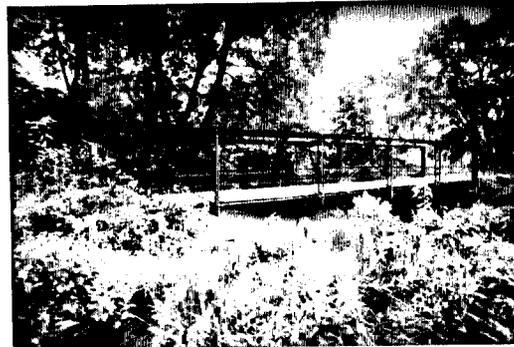
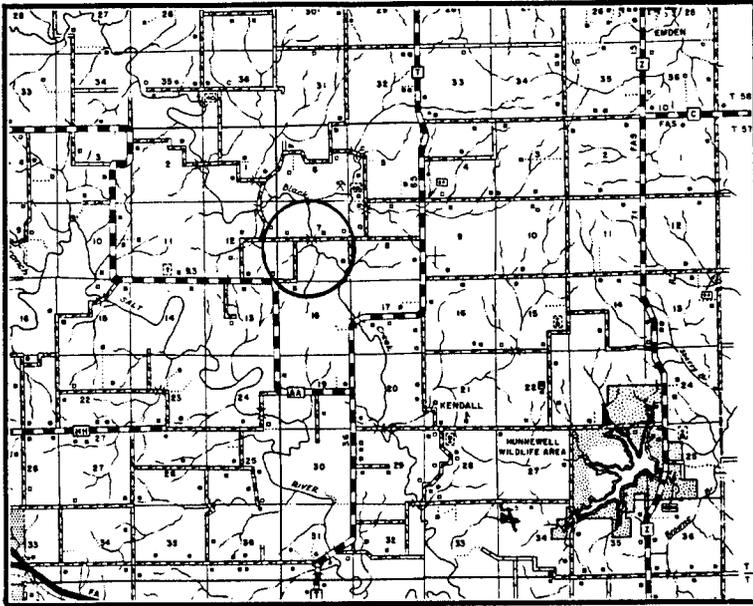
The Jordan Bridge carries a gravel-surfaced county road over Black Creek northeast of Shelbina, in southeastern Shelby County. The bridge is configured as a Pratt bedstead truss, with pinned connections and timber deck. In the summer of 1907, the Shelby County Court appropriated \$650.00 for this long-span bedstead, designed by county highway engineer W.W. Mitchell. The county contracted with the Michelmann Steel Construction Company of Quincy, Illinois, to fabricate and erect the structure, which was apparently completed later that year. Since its completion, the Jordan Bridge has continued to function in place, with the replacement of its bedstead legs with timber pile bents as the only alteration of note.

In a bedstead truss, a single "leg" functioned as both end post and support at each corner of the structure. This combined super- and substructure reduced erection costs somewhat, but bedsteads were prey to flood and collision damage and suffered from inherent structural weaknesses relating to compression stress in the lower chords. Despite these weaknesses, numerous truss leg bedsteads were erected throughout Missouri in the later 1890s and early 1900s. Hundreds remain in place today - in fact, Missouri has probably more bedsteads than any other state. The Jordan Bridge is distinguished as a well-preserved, long span example of this statewide bridge construction trend. In fact, its 80-foot length is unexcelled by any other bedstead in the state.

NAME(S) OF STRUCTURE

Jordan Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure No. 333000.5; Shelby County Court Record, Book 16: page 385 (4 May 1904); Shelby County Court Record, Book 17: page 307 (4 June 1907) - located at Shelby County Courthouse, Shelbyville MO; field inspection by Clayton Fraser, 8 October 1991.

INVENTORIED BY

Clayton B. Fraser

AFFILIATION

Fraserdesign, Loveland CO

DATE

3 February 1992
