

DAVISS COUNTY

INCLUDED: [Significant feature(s) of bridge given in boldface]
 [Field inventoried bridge indicated by asterisk]

Inv. No.	MHTD	Bridge Name	Description
DAVI01	H 752	Big Creek Ditch Bridge	1-140' riveted Pratt through truss 1928 Pioneer Construction Company
*DAVI02	J 146R	Grand River Bridge	3-140' riveted Pratt through truss 1930 Pioneer Construction Company
*DAVI03	017002.0	Cypress Creek Bridge	1- 40' riveted kingpost pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI04	025002.8	Hickory Creek Bridge	1- 25' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
DAVI05	044002.8	Little Creek Bridge	1- 40' steel stringer 1919
DAVI06	044003.6	Little Creek Bridge	1- 40' pinned Pratt pony truss 1909 John Dildine Bridge Company
DAVI07	052000.2	Tombstone Creek Bridge	1- 40' pinned Pratt bedstead c1915 John Dildine Bridge Co. (prob.)
DAVI08	065001.1	Hickory Creek Bridge	1- 32 riveted Pratt half-hip pony truss c1915 John Dildine Bridge Co. (prob.)
DAVI09	110R01.1	Muddy Creek Bridge	(replaced)
DAVI10	114000.5	Clear Creek Bridge	1- 40' pinned Pratt bedstead 1900 John Dildine Bridge Company
DAVI11	121001.8	Clear Creek Bridge	1- 60' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
*DAVI12	135000.2	Grand River Bridge	(replaced)
*DAVI13	154000.3	Lick Fork Bridge	1- 95' bowstring through arch-truss 1876 Missouri Valley B&I Company
DAVI14	172000.6	Bear Branch Bridge	1- 54' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI15	198001.7	Marrowbone Creek Bridge	1- 60' pinned Pratt bedstead 1900 John Dildine Bridge Company
DAVI16	207000.7	Dog Creek Bridge	1- 40' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI17	208001.1	Dog Creek Bridge	1- 50' pinned Pratt pony truss c1910 John Dildine Bridge Co. (prob.)
DAVI18	216000.6	Marrowbone Creek Bridge	(replaced)
*DAVI19	245000.5	Smith Branch Bridge	1- 30' pinned Pratt half-hip pony truss c1900 John Dildine Bridge Co. (prob.)
DAVI20	282000.7	Owl Creek Bridge	1- 40' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
DAVI21	289001.8	Little Creek Bridge	1- 40' pinned Pratt bedstead 1907 John Dildine Bridge Company
DAVI22	304001.0	Muddy Creek Bridge	1- 40' pinned Pratt bedstead c1910 John Dildine Bridge Co. (prob.)
*DAVI23	315003.4	Grindstone Creek Bridge	1-120' pinned Pratt through truss 1886 Missouri Valley B&I Works
*DAVI24	317000.8	Grand River Bridge	1-180' pinned Whipple through truss 1883 Kansas City B&I Company

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INCLUDED (cont.):

*DAVI25	321002.5	Grand River Bridge	1-178' 1892	pinned Pratt through truss John Dildine Bridge Company
*DAVI26	329001.1	Sampson Creek Bridge	1-140' c1910	pinned Camelback through truss John Dildine Bridge Co. (prob.)
*DAVI27	330000.9	Sampson Creek Bridge	1-100' 1893	pinned Pratt through truss John Dildine Bridge Company
*DAVI28	349003.1	Big Creek Bridge	1-102' c1910	pinned Pratt through truss John Dildine Bridge Co. (prob.)
*DAVI29	350001.2	Big Goose Creek Bridge	2- 60' c1910	pinned Pratt pony truss
*DAVI30	355002.4	Big Creek Bridge	1- 98' c1910	pinned Pratt through truss John Dildine Bridge Co. (prob.)
*DAVI31	359000.6	Big Creek Bridge	1-160' c1910	pinned Camelback through truss John Dildine Bridge Co. (prob.)
DAVI32	364000.9	Cypress Creek Bridge	1- 80' c1910	pinned Pratt pony truss John Dildine Bridge Co. (prob.)
DAVI33	380000.7	Hickory Creek Bridge	1- 50' 1916	pinned Pratt bedstead
DAVI34	396000.7	Pilot Grove Creek Bridge	1- 40' c1910	pinned Pratt pony truss John Dildine Bridge Company
DAVI35	420001.1	Big Muddy Creek Bridge	1- 40' c1910	pinned Pratt pony truss John Dildine Bridge Company
*DAVI36	422R01.0	Pilot Grove Creek Bridge	1- 60' 1947	2-angle Pratt pony truss Chillicothe Iron Works
DAVI37	435000.1	Bushy Creek Bridge	1- 23' 1918	steel stringer
DAVI38	455000.7	Big Muddy Creek Bridge	1-150' c1930	riveted Pratt through truss
DAVI39	458000.1	Grand River Bridge		(destroyed)
DAVI40	465000.6	Honey Creek Bridge	1- 40' 1907	pinned Pratt pony truss John Dildine Bridge Co. (prob.)
DAVI41	557001.0	Cypress Creek Bridge	1-102' c1930	riveted Pratt pony truss
DAVI42	574000.8	Big Muddy Creek Bridge		(replaced)
DAVI43	586000.0	Grand River Bridge		(destroyed)

EXCLUDED:

Pratt pony truss

033000.4 139001.1 283001.0 439001.2

Warren pony truss

H 751 J 860 006002.0 053000.1 220001.2 262001.5 269000.7
288001.9 384000.2 427000.9 565002.0

Warren bedstead

019000.2 157000.8 159000.8 197000.7 203000.3 217000.7 283002.9
366001.1 446001.0 566000.8

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EXCLUDED:

Steel stringer

F 100R1	J 986	K 169	K 170	K 184	S 182	S 791
S 792	S 793	T 895	T 896	X 117	X 733	X 968
002001.1	002001.5	007000.2	020R00.9	022000.7	022002.7	032000.5
045002.5	045004.1	049R00.3	052001.9	070001.4	092001.4	095001.7
097000.4	110002.8	116000.9	141000.0	146000.3	148001.0	157000.2
170001.2	197000.5	213R00.8	213000.5	215001.4	216002.2	232R01.2
251001.1	254001.6	256000.6	259R00.2	259R00.7	261000.7	266000.4
278000.4	283000.5	288001.2	289001.1	292000.6	293001.1	311R00.5
334001.2	350001.6	355000.3	370000.8	378002.0	384002.1	385001.5
393R00.6	395R00.5	401001.9	402001.0	408000.4	410000.6	414001.6
422000.8	424R00.3	431002.4	434000.1	434001.7	444000.1	446000.1
450001.0	459001.0	464001.5	475000.0	475001.8	477001.4	490R03.2
506000.3	514000.0	519R00.4	519000.6	525001.8	530000.1	555001.4
568000.4	592000.8	603001.0	615R01.0	615000.2	616000.4	

Steel girder

F1136R1	011001.3	375001.0	563001.3
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Concrete girder

F 102R1	S 442	364002.5
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Concrete slab

018002.4

Concrete box culvert

J 131	K 171	K 183	K 224	S 794	X 130	X 574
X 734	X 967					

Timber stringer

431000.8

SUMMARY:

	Primary	Secondary	Urban	Other	Total
Included	2	35	0	0	37
Excluded	28	112	0	0	140
	30	147	0	0	177 structures

Big Creek Ditch Bridge

DAVI01

GENERAL DATA

structure no.: H 752	city/town: Pattonsburg
county: Daviess	feature inters.: Big Creek Ditch
	cadastral grid: S26, T61N, R29W
	highway route: U.S. Highway 69
	highway distr.: 1
	current owner: Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure: steel, 7-panel, rigid-connected Pratt through truss, with rigid-connected Warren pony truss approach span	
substructure: concrete abutments, wingwalls and piers	
span number: 1	condition: good
span length: 140.0'	alterations: none
total length: 248.0'	floor/decking : concrete deck over steel stringers
roadway width: 20.0'	other features: steel pipe guardrails

HISTORICAL DATA

erection date: 1928	
erection cost: \$38,869.17	
designer: Missouri State Highway Department	
fabricator : unknown	
contractor: Pioneer Construction Company, Kansas City MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number H 752; Files on Primary System Bridges - located at the Missouri Highway and Transportation Department, Jefferson City MO.	
sign. rating: 41	
evaluation: NRHP non-eligible (typically configured example of highway truss construction)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI02

GENERAL DATA

structure no.:	J 146R	city/town:	1.8 miles south of Pattonsburg
county:	Daviess	feature inters.:	Grand River
		cadastral grid:	S11, T60N, R29W
		highway route:	U.S. Highway 69
		highway distr.:	1
		current owner:	Missouri Highway and Transportation Department

STRUCTURAL DATA

superstructure:	steel, 7-panel, rigid-connected Pratt through truss, with concrete girder approach spans		
substructure:	concrete abutments, wingwalls and piers		
span number:	3	condition:	good
span length:	140.0'	alterations:	deck repaired and guardrails replaced, 1990
total length:	617.0'	floor/decking :	asphalt / concrete deck over steel stringers
roadway width:	20.4'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 channels with batten plates, top and bottom; vertical: 4 angles with lacing; diagonal: 2 angles with batten plates; lateral bracing: 1 angle; strut: 2 angles with lacing; portal strut: 2 angles with lacing; guardrail: steel angle; bridge plate: Missouri Highway Dept. / Bridge N* J.146 / 1930

HISTORICAL DATA

erection date:	1930
erection cost:	\$77,658.25
designer:	Missouri State Highway Department
fabricator :	Inland Steel Company, East Chicago IN
contractor:	Pioneer Construction Company, Kansas City MO (1930); Chester Brothers Construction Company (1990)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number J 146R; Files on Primary System Bridges - located Ta the Missouri Highway and Transportation Department, Jefferson City MO; field inspection by Mitzi Rossillon, 13 September 1990.
sign. rating:	44
evaluation:	NRHP non-eligible (typically configured example of highway truss construction, somewhat notable for its multiple-span configuration)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI03

GENERAL DATA

structure no.: 017002.0	city/town: 3.1 miles northwest of Salem
county: Daviess	feature inters.: Cypress Creek
	cadastral grid: S3, T61N, R28W
	highway route: County Road 17
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected kingpost pony truss	
substructure: steel pile bent piers with timber back- and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: bridge closed
total length: 41.0'	floor/decking : timber deck over steel stringers
roadway width: 11.6'	other features: inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; guardrail: 2 angles

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : Cambria Steel Company, Pittsburgh PA	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 017002.0; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 53	
evaluation: NRHP possibly eligible (well-preserved atypically configured example of once-mainstay, now rare structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI04

GENERAL DATA

structure no.:	025002.8	city/town:	4.1 miles northeast of Salem
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S33, T62N, R27W
		highway route:	County Road 25
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 2-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber back- and wingwalls		
span number:	1	condition:	fair
span length:	25.0'	alterations:	unknown
total length:	25.0'	floor/decking :	timber deck
roadway width:	12.1'	other features:	unknown

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 025002.8.
sign. rating:	30
evaluation:	NRHP non-eligible (typically configured, inadequately documented example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI05

GENERAL DATA

structure no.:	044002.8	city/town:	9.4 miles east of Salem
county:	Daviess	feature inters.:	branch of Little Creek
		cadastral grid:	S4/33, T61/62N, R26W
		highway route:	County Road 44
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	concrete abutments and wingwalls	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	40.0'	other features:	steel angle guardrails
total length:	41.0'		
roadway width:	13.5'		

HISTORICAL DATA

erection date: 1919
erection cost: unknown
designer: unknown
fabricator : unknown
contractor: unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 044002.8.

sign. rating: 24
evaluation: NRHP non-eligible (typical example of exceedingly common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI06

GENERAL DATA

structure no.:	044003.6	city/town:	9.8 miles north of Jamesport
county:	Daviess	feature inters.:	Little Creek
		cadastral grid:	S3, T61N, R26W
		highway route:	County Road 44
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	53.0'	floor/decking :	timber deck
roadway width:	12.3'	other features:	unknown

HISTORICAL DATA

erection date:	1909
erection cost:	\$1677.00 (multiple-bridge contract amount)
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 044003.6; Daviess County Court Record, Book N: page 286 (1 February 1909) -located at Daviess County Courthouse, Gallatin MO.
sign. rating:	41
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Tombstone Creek Bridge

DAVI07

GENERAL DATA

structure no.:	052000.2	city/town:	10.6 miles north of Jamesport
county:	Daviess	feature inters.:	Tombstone Creek
		cadastral grid:	S34, T62N, R26W
		highway route:	County Road 52
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	48.0'	floor/decking :	timber deck
roadway width:	11.4'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1915
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 052000.2.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI08

GENERAL DATA

structure no.:	065001.1	city/town:	4.5 miles north of Jamesport
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S36, T61N, R26W
		highway route:	County Road 65
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, rigid-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	32.0'	alterations:	unknown
total length:	32.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	unknown

HISTORICAL DATA

erection date: c1915
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 065001.1.

sign. rating: 35
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Clear Creek Bridge

DAVI10

GENERAL DATA

structure no.: 114000.5	city/town: 3.2 miles north of Lock Springs
county: Daviess	feature inters.: Clear Creek
	cadastral grid: S25, T59N, R26W
	highway route: County Road 114
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans	
substructure: steel pile bent piers and abutments with timber back- and wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 122.0'	floor/decking : timber deck
roadway width: 11.5'	other features: steel angle guardrails

HISTORICAL DATA

erection date: 1900
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO
fabricator : John Dildine Bridge Company, Cameron MO
contractor: John Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 114000.5; Daviess County Court Record, Book L: page 251 (5 August 1900) - located at Daviess County Courthouse, Gallatin MO.

sign. rating: 41
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Clear Creek Bridge

DAVI11

GENERAL DATA

structure no.: 121001.8	city/town: 1.0 mile northwest of Lock Springs
county: Daviess	feature inters.: Clear Creek
	cadastral grid: S2/11, T58N, R26W
	highway route: County Road 121
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 4-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: concrete-filled steel cylinder piers	
span number: 1	condition: fair
span length: 60.0'	alterations: unknown
total length: 91.0'	floor/decking : timber deck
roadway width: 11.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 121001.8.	
sign. rating: 38	
evaluation: NRHP non-eligible (typically configured, inadequately documented example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Lick Fork Bridge

DAVI13

GENERAL DATA

structure no.:	154000.3	city/town:	4.6 miles southwest of Lock Springs
county:	Daviess	feature inters.:	Lick Fork
		cadastral grid:	S29, T58N, R26W
		highway route:	County Road 154
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 10-panel, bowstring through arch-truss, with steel stringer approach span

substructure: concrete abutments and iron pile pier

span number:	1	condition:	fair
span length:	95.0'	alterations:	none
total length:	110.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.5'	other features:	upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: star iron with star iron outrider with threaded ends; diagonal: round rods with threaded ends; strut: 1 channel; guardrail: 2 channels on main truss, 2 angles on approach span

HISTORICAL DATA

erection date: 1876

erection cost: \$2500.00 (probable)

designer: Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

fabricator : Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

contractor: Missouri Valley Bridge and Iron Company, Leavenworth KS (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 154000.3; Daviess County Court Record, Book F: page 382 (18 September 1876); Book J: page 191 (6 February 1893)- located at Daviess County Courthouse, Gallatin MO.

sign. rating: 76

evaluation: NRHP eligible (well-preserved example of once-mainstay, now rare structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Bear Branch Bridge

DAVI14

GENERAL DATA

structure no.:	172000.6	city/town:	7.6 miles south of Gallatin
county:	Daviess	feature inters.:	Bear Branch
		cadastral grid:	S27/28, T58N, R27W
		highway route:	County Road 172
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 4-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers with timber backwalls		
span number:	1	condition:	fair
span length:	54.0'	alterations:	unknown
total length:	54.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor :	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 172000.6.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Marrowbone Creek Bridge

DAVI15

GENERAL DATA

structure no.:	198001.7	city/town:	6.6 miles south of Altamont
county:	Daviess	feature inters.:	Marrowbone Creek
		cadastral grid:	S33, T58N, R28W
		highway route:	County Road 198
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	60.0'	alterations:	unknown
total length:	60.0'	floor/decking :	timber deck
roadway width:	11.5'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1900
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 198001.7; Daviess County Court Record, Book L: page 168 (4 June 1900) - located at Daviess County Courthouse, Gallatin MO.
sign. rating:	44
evaluation:	NRHP non-eligible (typical example of relatively common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Dog Creek Bridge

DAVI16

GENERAL DATA

structure no.:	207000.7	city/town:	3.2 miles south of Altamont
county:	Daviess	feature inters.:	Dog Creek
		cadastral grid:	S8, T58N, R28W
		highway route:	County Road 207
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach span		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	69.0'	floor/decking :	timber deck
roadway width:	13.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 207000.7.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Dog Creek Bridge

DAVI17

GENERAL DATA

structure no.:	208001.1	city/town:	3.3 miles south of Altamont
county:	Daviess	feature inters.:	Dog Creek
		cadastral grid:	S8/17, T58N, R28W
		highway route:	County Road 208
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	50.0'	floor/decking :	timber deck
roadway width:	13.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 208001.1.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Smith Branch Bridge

DAVI19

GENERAL DATA

structure no.: 245000.5	city/town: 4.7 miles southwest of Altamont
county: Daviess	feature inters.: branch of Smith Branch
	cadastral grid: S5, T58N, R29W
	highway route: County Road 245
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 2-panel, pin-connected Pratt half-hip pony truss
substructure: concrete abutments and wingwalls

span number: 1	condition: fair
span length: 30.0'	alterations: substructure repaired and beginning to collapse
total length: 31.0'	floor/decking : timber deck over steel and reinforcing timber stringers
roadway width: 11.4	other features: upper chord and inclined end post: 2 channels with lacing; lower chord: 2 looped square eyebars; vertical: 4 angles with lacing; diagonal: 2 looped round eyerods; lateral bracing: round rod with threaded ends; floor beam: I-beam, U-bolted to vertical; guardrail: 2 angles

HISTORICAL DATA

erection date: c1900
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : John Dildine Bridge Company, Cameron MO (probable)
contractor : John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 245000.5; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 31
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Owl Creek Bridge

DAVI20

GENERAL DATA

structure no.: 282000.7	city/town: 5.7 miles northwest of Altamont
county: Daviess	feature inters.: Owl Creek
	cadastral grid: S18/19, T59N, R29W
	highway route: County Road 282
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 56.0'	floor/decking : timber deck
roadway width: 11.6'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 282000.7.	
sign. rating: 35	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Little Creek Bridge

DAVI21

GENERAL DATA

structure no.:	289001.8	city/town:	5.9 miles south of Pattonsburg
county:	Daviess	feature inters.:	Little Creek
		cadastral grid:	S4, T59N, R29W
		highway route:	County Road 289
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	40.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	unknown

HISTORICAL DATA

erection date: 1907
erection cost: unknown
designer: John Dildine Bridge Company, Cameron MO
fabricator : John Dildine Bridge Company, Cameron MO
contractor: John Dildine Bridge Company, Cameron MO

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 289001.8; Daviess County Court Record, Book N: page 98 (7 October 1907) - located at Daviess County Courthouse, Gallatin MO.

sign. rating: 36
evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Muddy Creek Bridge

DAVI22

GENERAL DATA

structure no.:	304001.0	city/town:	3.9 miles southwest of Pattonsburg
county:	Daviess	feature inters.:	Muddy Creek
		cadastral grid:	S20, T60N, R29W
		highway route:	County Road 304
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt truss-leg bedstead, with steel stringer approach spans

substructure: steel pile bent piers and abutments with timber wingwalls

span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	68.0'	floor/decking :	timber deck
roadway width:	12.0'	other features:	unknown

HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: John Dildine Bridge Company, Cameron MO (probable)

fabricator : John Dildine Bridge Company, Cameron MO (probable)

contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 304001.0.

sign. rating: 35

evaluation: NRHP non-eligible (typical example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Grindstone Creek Bridge

DAVI23

GENERAL DATA

structure no.: 315003.4	city/town: 2.9 miles south of Pattonsburg
county: Daviess	feature inters.: Grindstone Creek
	cadastral grid: S15, T60N, R29W
	highway route: County Road 315
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: concrete-filled iron cylinder piers; timber pile bent abutments

span number: 1	condition: fair
span length: 120.0'	alterations: none
total length: 143.0'	floor/decking : timber deck over steel stringers
roadway width: 13.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyerods at the hip); diagonal: 2 punched rectangular eye-bars; counter: 2 round eyerods with turn-buckles; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: I-beam, U-bolted to vertical; guardrail: 1 channel; builder's plate: MO. VALLEY BRIDGE / & / IRON WORKS / 1886

HISTORICAL DATA

erection date: 1886
erection cost: \$2050.00 (contract amount)
designer: Missouri Valley Bridge and Iron Works, Leavenworth KS
fabricator : Missouri Valley Bridge and Iron Works, Leavenworth KS;
Carnegie Iron Works, Pittsburgh PA;
Illinois Steel Company, Chicago IL
contractor: Missouri Valley Bridge and Iron Works, Leavenworth KS

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 315003.4; Daviess County Court Record, Book H: page 453 (2 November 1885)- located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 56
evaluation: NRHP possibly eligible (well-preserved, relatively early example of main-stay structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI24

GENERAL DATA

structure no.: 317000.8	city/town: 1.8 miles south of Pattonsburg
county: Daviess	feature inters.: Grand River
	cadastral grid: S10, T60N, R29W
	highway route: County Road 317
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss; pin-connected Pratt through truss; 2 steel stringer approach spans

substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bent piers

span number: 1	condition: fair
span length: 180.0'	alterations: collision damage to guardrails
total length: 331.0'	floor/decking : timber deck over steel stringers
roadway width: 11.8'	other features: Whipple through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 angles; bridge plate: KANSAS CITY / BRIDGE & IRON CO./ 1883; Pratt through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 2 channels with spacers; portal strut: 4 angles with lacing; guardrail: 2 angles

HISTORICAL DATA

erection date: 1883

erection cost: unknown

designer: Kansas City Bridge and Iron Company, Kansas City MO

fabricator : Kansas City Bridge and Iron Company, Kansas City MO;
Carnegie Iron Works, Pittsburgh PA;
Cambria Iron Company, Pittsburgh PA

contractor: Kansas City Bridge and Iron Company, Kansas City MO

Grand River Bridge

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 317000.8; field inspection by Mitzi Rossillon, September 1990.

sign. rating: 78

evaluation: NRHP eligible (well-preserved, long-span example of rare early truss type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Grand River Bridge

DAVI25

GENERAL DATA

structure no.: 321002.5	city/town: 1.9 miles southwest of Pattonsburg
county: Daviess	feature inters.: Grand River
	cadastral grid: S9, T60N, R29W
	highway route: County Road 321
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss; 6 steel stringer approach spans at the west end and 2 steel stringer approach spans at the east end	
substructure: steel pile bent piers with timber wingwalls; concrete-filled iron cylinder piers	
span number: 1	condition: fair
span length: 178.0'	alterations: two approach spans, including abutments and piers, replaced
total length: 395.0'	floor/decking : timber deck over steel stringers
roadway width: 15.5'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars (4 rectangular eyebars on panels 4, 5 and 6); vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 channels (cable on east approach span)

HISTORICAL DATA

erection date: 1892	
erection cost: \$4970.00	
designer: John Dildine Bridge Company, Cameron MO	
fabricator : John Dildine Bridge Company, Cameron MO; Carnegie Iron Works, Pittsburgh PA	
contractor : John Dildine Bridge Company, Cameron MO	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 321002.5; Daviess County Court Record, Book J: page 204 (8 February 1893), page 81 (6 June 1892); Daviess County Court Record, Book N: pages 157-159 (4 February 1908), page 265 (7 December 1908) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 12 September 1990.	

Grand River Bridge

sign. rating: 53
evaluation: NRHP possibly eligible (relatively early, long-span example of mainstay structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Sampson Creek Bridge

DAVI26

GENERAL DATA

structure no.:	329001.1	city/town:	Pattonsburg
county:	Daviess	feature inters.:	Sampson Creek
		cadastral grid:	S27, T61N, R29W
		highway route:	County Road 329
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure: steel, 7-panel, pin-connected Camelback through truss
substructure: concrete abutments and wingwalls

span number:	1	condition:	fair
span length:	140.0'	alterations:	abutments replaced
total length:	140.0'	floor/decking :	concrete deck over steel stringers
roadway width:	14.7'	other features:	upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars, and 4 angles with batten plates; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eye-bars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: angles; portal strut: channels and angles; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910
erection cost: John Dildine Bridge Company, Cameron MO (probable)
designer: John Dildine Bridge Company, Cameron MO (probable)
fabricator : Illinois Steel Company, Chicago IL
contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 329001.1; field inspection by Mitzi Rossillon 9 September 1990.

sign. rating: 47
evaluation: NRHP possibly eligible (typically configured, though altered, example of uncommon structural type, only partially documented)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Sampson Creek Bridge

DAVI27

GENERAL DATA

structure no.:	330000.9	city/town:	1.0 mile northwest of Pattonsburg
county:	Daviess	feature inters.:	Sampson Creek
		cadastral grid:	S22, T61N, R29W
		highway route:	County Road 330
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end		
substructure:	steel pile bent piers and abutments		
span number:	1	condition:	fair
span length:	100.0'	alterations:	truss moved
total length:	132.0'	floor/decking :	timber deck over steel stringers
roadway width:	13.7'	other features:	upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square eyerods at the hip); diagonal: 2 looped rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles - top, round rods - bottom; strut: 4 angles with lacing; portal strut: 2 angles; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builders plate: JOHN DILDINE & CO / BRIDGE BUILDERS / CAMERON MO / 1893

HISTORICAL DATA

erection date:	1893
erection cost:	\$950.00
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor :	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 330000.9; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893); Daviess County Court Record, Book M: page 188 (5 July 1904) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 13 September 1990.
sign. rating:	39
evaluation:	NRHP non-eligible (typically configured example of mainstay structural type, moved to this location)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Creek Bridge

DAVI28

GENERAL DATA

structure no.: 349003.1	city/town: 3.8 miles north of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S11, T61N, R29W
	highway route: County Road 349
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans at each end	
substructure: concrete abutments; steel pile bent piers	
span number: 1	condition: fair
span length: 102.0'	alterations: substructure replaced
total length: 142.0'	floor/decking : timber deck over steel stringers; concrete deck on approach spans
roadway width: 15.8'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: A-frame; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor : John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349003.1; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 33	
evaluation: NRHP non-eligible (typical example of common structural type, altered and poorly documented)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Goose Creek Bridge

DAVI29

GENERAL DATA

structure no.: 350001.2	city/town: 4.5 miles north of Pattonsburg
county: Daviess	feature inters.: Big Goose Creek
	cadastral grid: S3, T61N, R29W
	highway route: County Road 350
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 5-panel, rigid-connected Pratt pony truss, with pin-connected Pratt half-hip pony approach span at the east end and steel stringer approach span at the west end	
substructure: concrete abutments; timber wingwalls; steel pile bent piers with concrete caps	
span number: 2	condition: fair
span length: 60.0'	alterations: trusses moved to this location
total length: 104.0'	floor/decking : concrete deck over steel stringers
roadway width: 13.0'	other features: rigid-connected Pratt pony: upper chord and inclined end post: I-beam; lower chord: 2 angles with batten plates; vertical: 4 angles with batten plates; diagonal: 2 angles with batten plates; counter: 2 angles with batten plates; lateral bracing: round rod with threaded ends; pin- and rigid-connected Pratt pony: upper chord and inclined end post: 2 channels with lacing; lower chord: 2 angles; vertical: 4 angles with lacing; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: 2 channels

HISTORICAL DATA

erection date: c1920	
erection cost: unknown	
designer: unknown	
fabricator : Illinois Steel Company, Chicago IL; Jones and Laughlin Steel Company, Pittsburgh PA	
contractor: unknown	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 350001.2; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 24	
evaluation: NRHP non-eligible (poorly preserved, poorly documented example of common structural type)	
inventoried by: Michelle Crow-Dolby	2 April 1993

Big Creek Bridge

DAVI30

GENERAL DATA

structure no.: 355002.4	city/town: 1.6 miles north of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S23, T61N, 29RW
	highway route: County Road 355
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt through truss, with steel stringer approach span at each end

substructure: concrete abutments and steel pile bent piers

span number: 1	condition: fair
span length: 98.0'	alterations: 1 set of hip verticals repaired; bottom lateral brace broken
total length: 138.0'	floor/decking : timber deck over steel stringers
roadway width: 11.6'	other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: 4 angles with lacing; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels

HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: John Dildine Bridge Company, Cameron MO (probable)

fabricator : John Dildine Bridge Company, Cameron MO (probable); Illinois Steel Company, Chicago IL

contractor : John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 355002.4; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 35

evaluation: NRHP non-eligible (typically configured, poorly documented example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Creek Bridge

DAVI31

GENERAL DATA

structure no.: 359000.6	city/town: immediately east of Pattonsburg
county: Daviess	feature inters.: Big Creek
	cadastral grid: S36, T61N, R29W
	highway route: County Road 359
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 8-panel, pin-connected Camelback through truss, with steel stringer approach span at the west end

substructure: concrete abutments, wingwalls and piers

span number: 1	condition: fair
span length: 160.0'	alterations: substructure replaced
total length: 180.0'	floor/decking : concrete deck over steel stringers
roadway width: 15.7'	other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (4 angles with batten plates at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: 2 angles; guardrail: 2 angles (south side of west approach span is cable)

HISTORICAL DATA

erection date: c1910

erection cost: unknown

designer: John Dildine Bridge Company, Cameron MO (probable)

fabricator : John Dildine Bridge Company, Cameron MO (probable);
Inland Steel Company, East Chicago IN

contractor: John Dildine Bridge Company, Cameron MO (probable)

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.6; field inspection by Mitzi Rossillon, 9 September 1990.

sign. rating: 50

evaluation: NRHP possibly eligible (typically configured, though altered, example of uncommon structural type, only partially documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI32

GENERAL DATA

structure no.: 364000.9	city/town: 3.9 miles northeast of Pattonsburg
county: Daviess	feature inters.: Cypress Creek
	cadastral grid: S20, T61N, R28W
	highway route: County Road 364
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 5-panel, pin-connected Pratt pony truss	
substructure: unknown	
span number: 1	condition: fair
span length: 80.0'	alterations: unknown
total length: 80.0'	floor/decking : timber deck
roadway width: 11.7'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor: John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 364000.9.	
sign. rating: 40	
evaluation: NRHP non-eligible (typical example of common truss type)	

Inventoried by: Michelle Crow-Dolby 2 April 1993

Hickory Creek Bridge

DAVI33

GENERAL DATA

structure no.:	380000.7	city/town:	1.4 miles southeast of Salem
county:	Daviess	feature inters.:	Hickory Creek
		cadastral grid:	S7, T61N, R27W
		highway route:	County Road 380
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt truss-leg bedstead		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	50.0'	alterations:	unknown
total length:	69.0'	floor/decking :	timber deck
roadway width:	11.7'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	1916
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 380000.7.

sign. rating:	32
evaluation:	NRHP non-eligible (typical example of common structural type, poorly documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

Pilot Grove Creek Bridge

DAVI34

GENERAL DATA

structure no.:	396000.7	city/town:	7.2 miles southeast of Salem
county:	Daviess	feature inters.:	Pilot Grove Creek
		cadastral grid:	S25, T61N, R27W
		highway route:	County Road 396
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans		
substructure:	steel pile bent piers with timber wingwalls		
span number:	1	condition:	fair
span length:	40.0'	alterations:	unknown
total length:	84.0'	floor/decking :	timber deck
roadway width:	11.9'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1910
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO (probable)
fabricator :	John Dildine Bridge Company, Cameron MO (probable)
contractor:	John Dildine Bridge Company, Cameron MO (probable)
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 396000.7.
sign. rating:	35
evaluation:	NRHP non-eligible (typical example of common structural type)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Big Muddy Creek Bridge

DAVI35

GENERAL DATA

structure no.: 420001.1	city/town: 1.9 miles southeast of Jameson
county: Daviess	feature inters.: Big Muddy Creek
	cadastral grid: S29, T60N, R27W
	highway route: County Road 420
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 3-panel, pin-connected Pratt pony truss, with steel stringer approach spans	
substructure: steel pile bent piers with timber wingwalls	
span number: 1	condition: fair
span length: 40.0'	alterations: unknown
total length: 60.0'	floor/decking : timber deck
roadway width: 15.4'	other features: steel angle guardrails

HISTORICAL DATA

erection date: c1910	
erection cost: unknown	
designer: John Dildine Bridge Company, Cameron MO (probable)	
fabricator : John Dildine Bridge Company, Cameron MO (probable)	
contractor : John Dildine Bridge Company, Cameron MO (probable)	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 420001.1.	
sign. rating: 35	
evaluation: NRHP non-eligible (typical example of common structural type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Pilot Grove Creek Bridge

DAVI36

GENERAL DATA

structure no.: 422R01.0	city/town: 3.5 miles southeast of Jameson
county: Daviess	feature inters.: Pilot Grove Creek
	cadastral grid: S28/33, T60N, R27W
	highway route: County Road 422
	highway distr.: 1
	current owner: Daviess County

STRUCTURAL DATA

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss, with steel stringer approach spans	
substructure: timber abutments; steel pile bent piers	
span number: 1	condition: fair
span length: 60.0'	alterations: abutments replaced
total length: 114.0'	floor/decking : timber deck over alternating steel and timber stringers
roadway width: 15.5	other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: channel and angle

HISTORICAL DATA

erection date: 1947 (probable)	
erection cost: unknown	
designer: Chillicothe Iron Works, Chillicothe MO (probable)	
fabricator : Chillicothe Iron Works, Chillicothe MO (probable)	
contractor : county crew	
references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 422R01.0; field inspection by Mitzi Rossillon, 13 September 1990.	
sign. rating: 37	
evaluation: NRHP non-eligible (undistinguished example of uncommon truss type)	

inventoried by: Michelle Crow-Dolby 2 April 1993

Brushy Creek Bridge

DAVI37

GENERAL DATA

structure no.:	435000.1	city/town:	4.0 miles southwest of Jamesport
county:	Daviess	feature inters.:	branch of Brushy Creek
		cadastral grid:	S1/6, T59N, R26/27W
		highway route:	County Road 435
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel stringer	condition:	fair
substructure:	unknown	alterations:	unknown
span number:	1	floor/decking :	unknown
span length:	24.0'	other features:	unknown
total length:	24.0'		
roadway width:	16.5'		

HISTORICAL DATA

erection date:	1918
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 435000.1.
sign. rating:	24
evaluation:	NRHP non-eligible (typical, small-scale example of common structural type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Big Muddy Creek Bridge

DAVI38

GENERAL DATA

structure no.:	455000.7	city/town:	2.3 miles west of Carlow
county:	Daviess	feature inters.:	Big Muddy Creek
		cadastral grid:	S36, T59N, R27W
		highway route:	County Road 455
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 9-panel, rigid-connected Pratt through truss, with steel stringer approach spans		
substructure:	unknown		
span number:	1	condition:	fair
span length:	150.0'	alterations:	unknown
total length:	196.0'	floor/decking :	timber deck
roadway width:	15.8'	other features:	steel angle guardrails

HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor:	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 455000.7.

sign. rating:	28
evaluation:	NRHP non-eligible (typical example of common truss type, poorly documented)

Inventoried by: Michelle Crow-Dolby 2 April 1993

Honey Creek Bridge

DAVI40

GENERAL DATA

structure no.:	465000.6	city/town:	2.7 miles south of Gallatin
county:	Daviess	feature inters.:	Honey Creek
		cadastral grid:	S31, T59N, R27W
		highway route:	County Road 465
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 3-panel, pin-connected Pratt pony truss		
substructure:	concrete-filled steel cylinder piers		
span number:	1	condition:	fair
span length:	40.0'	alterations:	none
total length:	80.0'	floor/decking :	timber deck
roadway width:	11.8'	other features:	steel angle guardrails; laced endposts

HISTORICAL DATA

erection date:	1907
erection cost:	unknown
designer:	John Dildine Bridge Company, Cameron MO
fabricator :	John Dildine Bridge Company, Cameron MO
contractor:	John Dildine Bridge Company, Cameron MO
references:	Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 465000.6; Daviess County Court Record, Book N: page 10 (5 February 1907) - located at Daviess County Courthouse, Gallatin MO.
sign. rating:	43
evaluation:	NRHP non-eligible (typical example of common truss type)

inventoried by: Michelle Crow-Dolby 2 April 1993

Cypress Creek Bridge

DAVI41

GENERAL DATA

structure no.:	557001.0	city/town:	3.4 miles east of Pattonsburg
county:	Daviess	feature inters.:	Cypress Creek
		cadastral grid:	S29, T61N, R28W
		highway route:	County Road 557
		highway distr.:	1
		current owner:	Daviess County

STRUCTURAL DATA

superstructure:	steel, 5-panel, rigid-connected Pratt pony truss		
substructure:	unknown		
span number:	1	condition:	fair
span length:	102.0'	alterations:	unknown
total length:	103.0'	floor/decking :	timber deck
roadway width:	16.3'	other features:	unknown

HISTORICAL DATA

erection date:	c1930
erection cost:	unknown
designer:	unknown
fabricator :	unknown
contractor :	unknown

references: Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 557001.0.

sign. rating:	28
evaluation:	NRHP non-eligible (long-span example of common truss type, poorly documented)

inventoried by: Michelle Crow-Dolby 2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Cypress Creek Bridge
MHTD: 017002.0

DAVI03

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 17 over Cypress Creek; S3, T61N, R28W
3.1 miles northwest of Salem; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / abandoned

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Daviess County

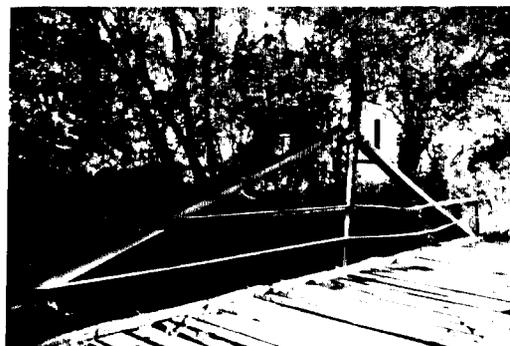
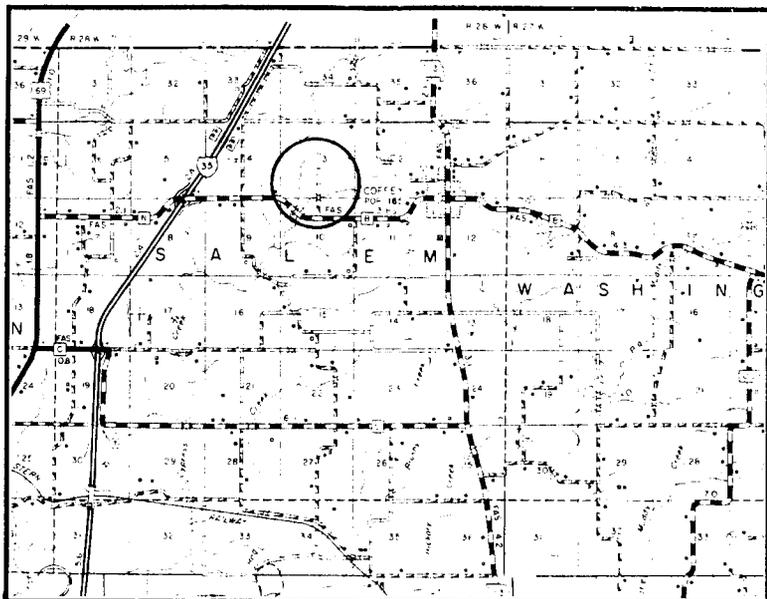
span number: 1
span length: 40.0'
total length: 41.0'
roadway wdt.: 11.6'

superstructure: steel, 2-panel, rigid-connected kingpost pony truss
substructure: steel pile bent piers with timber back- and wingwalls
floor/decking: timber deck over steel stringers
other features: inclined end post: I-beam; lower chord: 2 channels with batten plates; vertical: 4 angles with lacing; lateral bracing: round rod with threaded ends; guardrail: 2 angles

Located about 3.1 miles northwest of Salem, the small-scale truss bridge spans Cypress Creek on an abandoned section of ungraded county road. The structure is comprised of a single rigid-connected kingpost pony truss supported by steel pile bent abutments with timber backwalls. Made up of steel components rolled in Pittsburgh by Cambria, the truss is atypically configured, with steel I-beams used in lieu of built-up box beams for the inclined endposts. No specific reference to this bridge has been found in Daviess County records. It was most likely fabricated and built by the John Dildine Bridge Company of Cameron, Missouri, Daviess County's perennial contractor in the early 20th century. Other than the bridge's closure to vehicular traffic, it appears in essentially unaltered condition today.

NAME(S) OF STRUCTURE
Cypress Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 017002.0; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

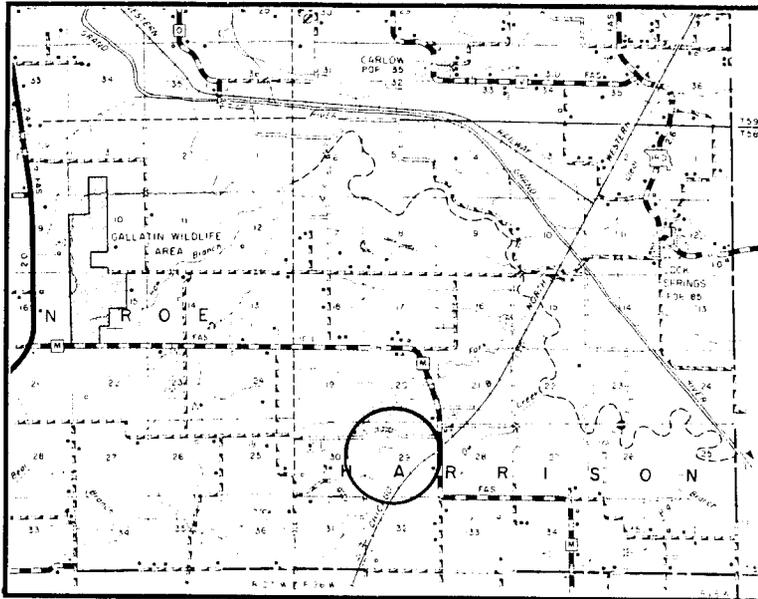
DATE

2 April 1993

NAME(S) OF STRUCTURE

Lick Fork Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 154000.3; Daviess County Court Record, Book F: page 382 (18 September 1876); Book J: page 191 (6 February 1893)- located at Daviess County Courthouse, Gallatin MO.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Lick Fork Bridge
MHTD: 154000.3

DAVI13

DATE(S) OF CONSTRUCTION

1876

LOCATION

County Road 154 over Lick Fork; S29, T58N, R26W
4.6 miles southwest of Lock Springs; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 76)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 95.0'
total length: 110.0'
roadway wdt.: 13.5'

superstructure: wrought iron, 10-panel, bowstring through arch-truss, with steel stringer approach span
substructure: concrete abutments and iron pile pier
floor/decking: timber deck over steel stringers
other features: upper chord: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: star iron with star iron outrider with threaded ends; diagonal: round rods with threaded ends; strut: 1 channel; guardrail: 2 channels on main truss, 2 angles on approach span

Designed and built by the Missouri Valley Bridge and Iron Company of Leavenworth, Kansas, the Lick Fork Creek Bridge features a timber deck over steel stringers and is supported by a steel stringer approach span on its north end. In September of 1876 the Daviess County Court let a contract to design and build a bridge in Section 29, Township 58, Range 26. Total estimated cost of the bridge: \$2500.00. County records indicate that seventeen years later, in 1893, James Burns was paid \$160.00 for "replacing the bridge's chords, needle beams, five or more joists, one piling and the flooring." The bowstring through arch truss is situated in southeastern Daviess County approximately five miles southwest of Lock Springs. Since its 1876 completion the bridge has carried County Road 154 across the creek in this sparsely populated setting.

The bowstring arch-truss was the iron span of choice for Missouri counties in the late 1860s and 1870s. Marketed exclusively throughout the Midwest by such industry giants as the King Iron Bridge and Manufacturing Company and the Wrought Iron Bridge Company, these often-patented bridge forms featured a wide range of span lengths, economical fabrication cost and relatively quick erection. The proliferation of the bowstring corresponded with the initial development of Missouri's road system; as a result, perhaps thousands of these prototypical iron spans erected throughout the state. The bowstring had some rather severe structural flaws, however, relating primarily to lateral stability of the arches, and it was largely superseded by the pin-connected truss in the early 1880s. Despite this, some bowstrings were still erected in Missouri in the 1880s, although the number dwindled precipitously by the decade's end. Through subsequent attrition, almost all of Missouri's bowstrings have been demolished and replaced. Now less than twenty remain in place. The Lick Fork Creek Bridge is thus historically significant as one of the last remaining examples in the state of what was once a mainstay structural type.

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grindstone Creek Bridge
MHTD: 315003.4

DAVI23

DATE(S) OF CONSTRUCTION

1886

LOCATION

County Road 315 over Grindstone Creek; S15, T60N, R29W
2.9 miles south of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 56)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 120.0'
total length: 143.0'
roadway wdt.: 13.7'

superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: concrete-filled iron cylinder piers; timber pile bent abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round eyerods at the hip); diagonal: 2 punched rectangular eyebars; counter: 2 round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: I-beam; floor beam: I-beam, U-bolted to vertical; guardrail: 1 channel; builder's plate: **MO. VALLEY BRIDGE / & / IRON WORKS / 1886**

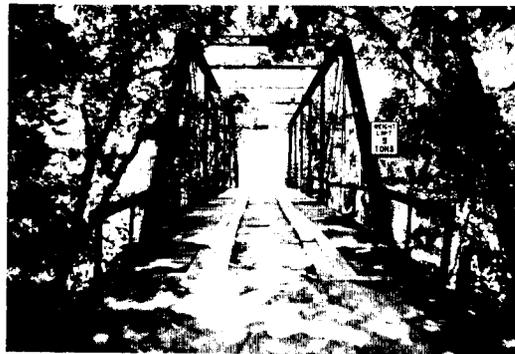
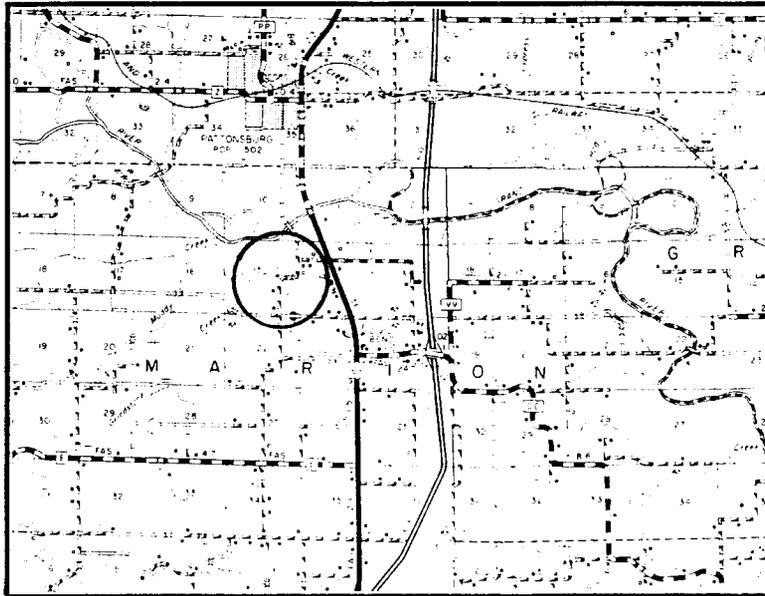
This medium-span truss carries an unpaved county road over Grindstone Creek, south of Pattonsburg in western Daviess County. The structure consists of a single pin-connected Pratt through truss, supported by iron cylinder piers and approached by steel stringers on both sides. County records indicate that a construction contract was let to the Missouri Valley Bridge Company of Leavenworth, Kansas, for the inclusive amount of \$2050.00. The Carnegie Iron Company, based out of Pittsburgh, furnished the wrought iron components for the truss. The bridge, according to contractual agreement, was to be completed on or before February 1886. The Grindstone Creek Bridge has carried inter-county traffic since, with the modification of its guardrails as the only alteration of note.

From the 1870s through the 1910s, thousands of pin-connected Pratt through trusses were erected on Missouri's roadways. Although several hundred of these structures remain in use today, relatively few date from before the turn of the century. And fewer yet of the 19th century survivors remain structurally intact. The Grindstone Creek Bridge stands out among Missouri's pin-connected trusses as an early, fairly well-documented example that has retained a high degree of structural integrity.

NAME(S) OF STRUCTURE

Grindstone Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 315003.4; Daviess County Court Record, Book H: page 453 (2 November 1885)- located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 9 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grand River Bridge
MHTD: 317000.8

DAVI24

DATE(S) OF CONSTRUCTION

1883

LOCATION

County Road 317 over Grand River; S10, T60N, R29W
1.8 miles south of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP eligible (score: 78)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 180.0'
total length: 331.0'
roadway wdt.: 11.8'

superstructure: wrought iron, 11-panel, pin-connected Whipple through truss; pin-connected Pratt through truss; 2 steel stringer approach spans
substructure: concrete abutments and wingwalls; concrete-filled iron cylinder piers; steel pile bent piers
floor/decking: timber deck over steel stringers
other features: Whipple through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 square rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 angles; bridge plate: **KANSAS CITY / BRIDGE & IRON CO./ 1883**; Pratt through truss: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars; vertical: 2 channels with lacing (2 round rods at the hip); diagonal: 2 punched rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles; strut: 2 channels with spacers; portal strut: 4 angles with lacing; guardrail: 2 angles

This long-span combination Whipple and Pratt through truss carries a gravel-surfaced county road across the Grand River about two miles south of Pattonsburg, in northwestern Daviess County. The Grand River Bridge consists of a pin-connected Whipple through truss and a Pratt through truss supported by steel pile bent piers and concrete-filled steel cylinder piers. The bridge was fabricated and erected by the Kansas City Bridge and Iron Company, using wrought iron components rolled by Carnegie Iron Works Company and Cambria Iron Company, both of Pittsburgh, Pennsylvania. Built at an unknown cost, the bridge was completed in 1883. Barring minor collision damage, the truss retains its historical and structural integrity in its original heavily wooded setting.

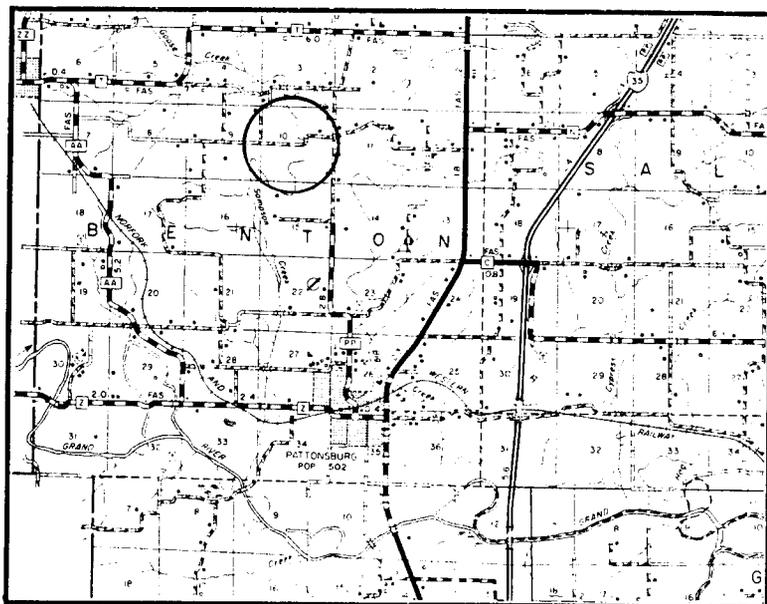
Configured as a pin-connected Whipple (or double-intersection Pratt) through truss, the Grand River Bridge is one of less than ten such structures known to exist in Missouri. First patented by Squire Whipple, bridge engineer and builder from New York, the Whipple truss was a popular choice for long-span crossings between 1850 and 1890. The Whipple truss differed from the more common Pratt in that its diagonal members extended across, not one, but two panels. Although more costly, this variation provided greater lateral support

connection to the floor and lateral systems." Virtually all of the regional bridge fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties. The John Dildine Bridge Company was a major player in Missouri during the late 19th century, and this structure represents that company's penchant for pinned truss construction. With an erection date of 1892, the Grand River Bridge is distinguished as one of the oldest originally placed Pratt trusses in Missouri. It is thus technologically and historically significant, well-preserved, transportation-related resource, one of the most important of the state's early spans.

NAME(S) OF STRUCTURE

Grand River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 317000.8; field inspection by Mitzi Rossillon, September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Grand River Bridge
MHTD: 321002.5

DAVI25

DATE(S) OF CONSTRUCTION

1892

LOCATION

County Road 321 over Grand River; S9, T60N, R29W
1.9 miles southwest of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 53)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 178.0'
total length: 395.0'
roadway wdt.: 15.5'

superstructure: wrought iron, 9-panel, pin-connected Pratt through truss; 6 steel stringer approach spans at the west end and 2 steel stringer approach spans at the east end
substructure: steel pile bent piers with timber wingwalls; concrete-filled iron cylinder piers
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 punched rectangular eyebars (4 rectangular eyebars on panels 4, 5 and 6); vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 4 angles with lacing; portal strut: 4 angles with lacing; guardrail: 2 channels (cable on east approach span)

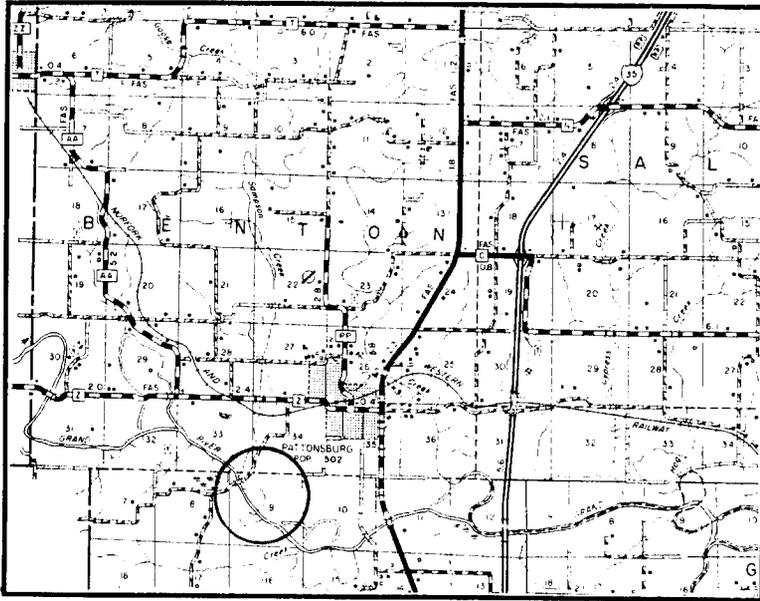
Located near Pattonsburg in western Daviess County, the Grand River Bridge features a pinned Pratt through truss with six steel stringer approach spans on its west end and two steel stringer approach spans on its east end. Supported by steel pile bent and tubular piers, this long-span bridge dates to 1892. The Daviess County Court that year awarded a contract to the John Dildine Bridge Company of Cameron, Missouri, to fabricate and erect the bridge for \$4970.00, which was to be shared equally between subscribers and the county. In 1908, several years after its completion, the county again contracted Dildine to do repair work on the bridge's main span and approaches. One of several pinned Pratt through trusses included in the county's historic bridge inventory, the Grand River Bridge is one of the longest. Its substructure and approach spans have apparently been replaced, but the truss itself remains intact.

In the early 1880s, the pin-connected Pratt truss superseded the bowstring arch-truss as the iron bridge of choice for medium-span wagon crossings. Patented in 1844 by Thomas and Caleb Pratt, the Pratt design is distinguished by vertical members acting in compression and diagonals that act in tension. "The Pratt truss is the type most commonly used in America for spans under two hundred and fifty (250) feet in length," noted bridge engineer J.A.L. Waddell in 1916. "Its advantages are simplicity, economy of metal, and suitability for connection to the floor and lateral systems." Virtually all of the regional bridge fabricators manufactured Pratt trusses and marketed them extensively to Missouri's counties. The John Dildine Bridge Company was a major player in Missouri during the late 19th century, and this structure represents that company's penchant for pinned truss construction. With an erection date of 1892, the Grand River Bridge is distinguished as one of the oldest originally placed Pratt trusses in Missouri. It is thus a technologically and historically significant, transportation-related resource.

NAME(S) OF STRUCTURE

Grand River Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 321002.5; Daviess County Court Record, Book J: page 204 (8 February 1893), page 81 (6 June 1892); Daviess County Court Record, Book N: pages 157-159 (4 February 1908), page 265 (7 December 1908) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 12 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Sampson Creek Bridge
MHTD: 329001.1

DAVI26

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 329 over Sampson Creek; S27, T61N, R29W
Pattonburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 47)

CONDITION

fair

OWNER

Daviess County

span number: 1

span length: 140.0'

total length: 140.0'

roadway wdt.: 14.7'

superstructure: steel, 7-panel, pin-connected Camelback through truss

substructure: concrete abutments and wingwalls

floor/decking: concrete deck over steel stringers

other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 punched rectangular eyebars, and 4 angles with batten plates; vertical: 2 channels with lacing; diagonal: 2 punched rectangular eyebars; counter: round eyerod with turnbuckle; lateral bracing: round rod with threaded ends; strut: angles; portal strut: channels and angles; guardrail: 2 channels

This single-span Camelback through truss over Sampson Creek features a 140-foot span length. Located in northwestern Daviess County in the town of Pattonburg, the Sampson Creek Bridge features a concrete deck over steel stringers and concrete abutments. The bridge construction company of choice in Daviess County, John Dildine Bridge Company of Cameron, Missouri, was probably contracted around 1910 to fabricate and erect the bridge. At some later time the original abutments were removed and new concrete abutments were installed. Other than this minor alteration, the structure remains structurally intact and continues to carry intermittent vehicular traffic.

Straight-chorded Pratt through trusses were used extensively throughout Missouri for medium-span crossings in the late 19th and early 20th centuries. For longer crossings after about the turn of the century, however, bridge companies could develop greater efficiency with polygonal-chorded Pratt variants - primarily Parker, Pennsylvania and Camelback trusses. With its distinctive five-faceted upper chords, the Camelback configuration was disdained by some engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on their individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and less than ten have been identified as extant by the bridge inventory. The Sampson Creek Bridge is thus technologically significant as a well-preserved example of this uncommon structural type.

INVENTORIED BY
Michelle Crow-Dolby

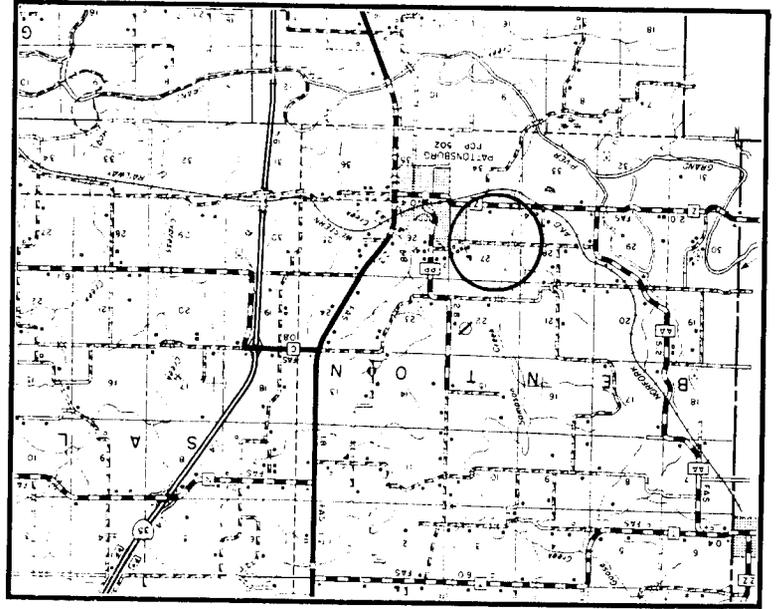
AFFILIATION
Fraserdesign, Loveland CO

DATE
2 April 1993

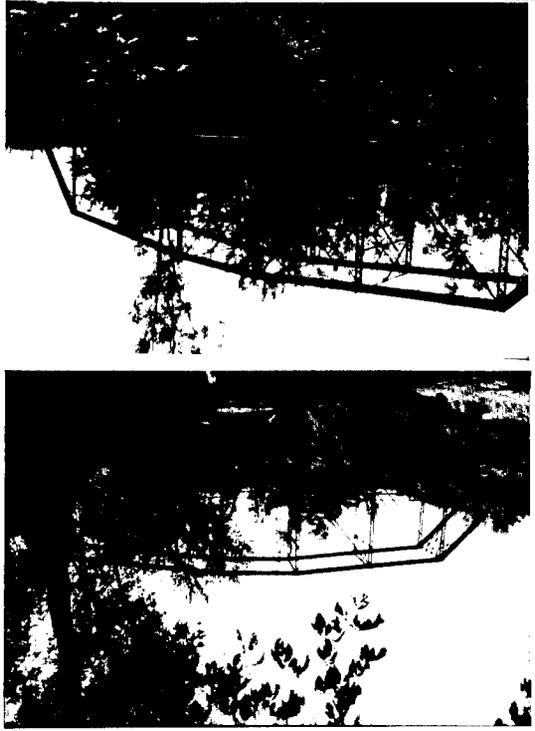
SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 329001.1; field inspection by
Mitzi Rossillon 9 September 1990.

LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



NAME(S) OF STRUCTURE
Sampson Creek Bridge
PHOTOS AND SKETCH MAP OF LOCATION



HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Sampson Creek Bridge
MHTD: 330000.9

DAVI27

DATE(S) OF CONSTRUCTION

1893

LOCATION

County Road 330 over Sampson Creek; S22, T61N, R29W
1.0 mile northwest of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 39)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 100.0'
total length: 132.0'
roadway wdt.: 13.7'

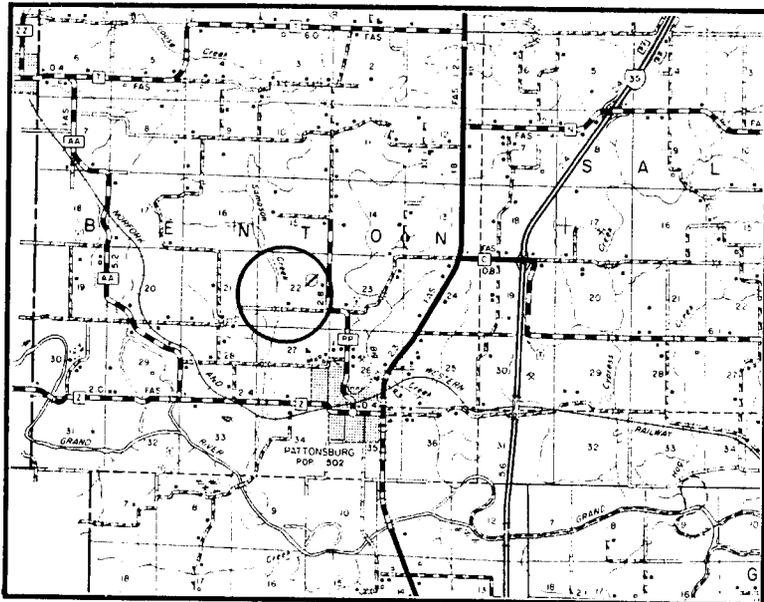
superstructure: wrought iron, 7-panel, pin-connected Pratt through truss, with steel stringer approach span at each end
substructure: steel pile bent piers and abutments
floor/decking: timber deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover and batten plates; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (2 square eyerods at the hip); diagonal: 2 looped rectangular eyerods; counter: round rods with turnbuckles; lateral bracing: round rods with turnbuckles - top, round rods - bottom; strut: 4 angles with lacing; portal strut: 2 angles; floor beam: I-beam, U-bolted to vertical; guardrail: 2 channels; builders plate: JOHN DILDINE & CO / BRIDGE BUILDERS / CAMERON MO / 1893

Competitive bids were solicited by the Daviess County Court for a bridge across Sampson Creek in August 1893. After reviewing all the bids, the judges signed a contract with John Dildine Bridge Company of Cameron, Missouri, on September 5, 1893, to fabricate and erect the Pratt through truss. Total cost: \$950.00. Completed in 1893, the Sampson Creek Bridge continues to carry vehicular traffic in northwestern Daviess County. Both county records along with the bridge's contemporary bent piers and approach spans indicate that the truss is not in its original locale. Although documentation does not refer to this relocation specifically, the truss might have been moved from a site over Big Creek near Pattonsburg. The bridge, despite its probable transfer and modern features, retains an average degree of physical integrity. It currently carries daily traffic on County Road 330.

Having long since acquired a sense of time and place at its current location northwest of Pattonsburg, the Sampson Creek Bridge is a well-preserved and fairly well-documented example of a mainstay structural type - the Pratt through truss.

NAME(S) OF STRUCTURE

Sampson Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION**LOCATION MAP**TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 330000.9; Daviess County Court Record, Book J: page 276 (7 August 1893), page 303 (5 September 1893); Daviess County Court Record, Book M: page 188 (5 July 1904) - located at Daviess County Courthouse, Gallatin MO; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 349003.1

DAVI28

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 349 over Big Creek; S11, T61N, R29W
3.8 miles north of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 33)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 102.0'
total length: 142.0'
roadway wdt.: 15.8'

superstructure: steel, 6-panel, pin-connected Pratt through truss, with steel stringer approach spans at each end
substructure: concrete abutments; steel pile bent piers
floor/decking: timber deck over steel stringers; concrete deck on approach spans
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 rectangular eyebars; vertical: 2 channels with lacing (2 square eyebars at the hip); diagonal: 2 punched rectangular eyebars; counter: round rods with turnbuckles; lateral bracing: round rods with threaded ends; strut: 2 angles; portal strut: A-frame; guardrail: 2 channels

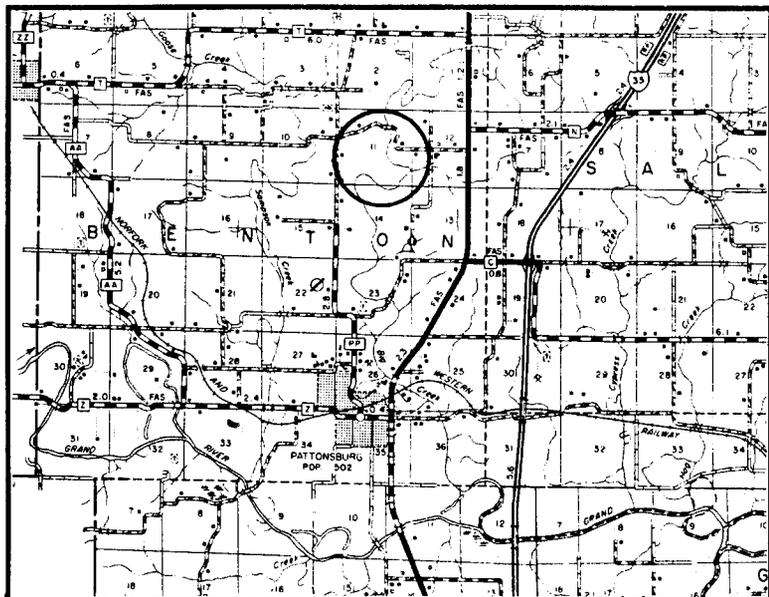
Among the Daviess County bridges in Missouri's historic bridge inventory is this short-span through truss. Located about four miles north of Pattonsburg, the structure carries County Road 349 over Big Creek. The bridge consists of a pinned Pratt through truss with portal A-frame bracing and a timber deck over steel stringers. Two steel stringer spans, supported by steel pile bents, form approaches on each end. Around 1910 the locally popular contractor, John Dildine Bridge Company of Cameron, Missouri, was probably awarded a contract to erect a bridge at this location. The Big Creek Bridge has carried traffic in north-western Daviess County since, with the replacement of its approach spans as well as its pile bents the only alterations of note.

In Missouri the pinned Pratt through truss was the bridge of choice for short- and medium-span applications in the late 19th and early 20th centuries. Most of the structures erected during this period were based on standard plans developed by either the state highway department (after 1917) or by the individual bridge companies, such as the prolific John Dildine Bridge Company, based out of Cameron. As a result, thousands of Pratts were built across the state, all very much alike in detailing, and today the Pratt truss constitutes the most populous group of through trusses. The Big Creek Bridge is a typically configured pin-connected Pratt through truss.

NAME(S) OF STRUCTURE

Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 349003.1; field inspection by Mitzi Rossillon, 13 September 1990.

INVENTORIED BY

Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Big Creek Bridge
MHTD: 359000.6

DAVI31

DATE(S) OF CONSTRUCTION

c1910

LOCATION

County Road 359 over Big Creek; S36, T61N, R29W
immediately east of Pattonsburg; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP possibly eligible (score: 50)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 160.0'
total length: 180.0'
roadway wdt.: 15.7'

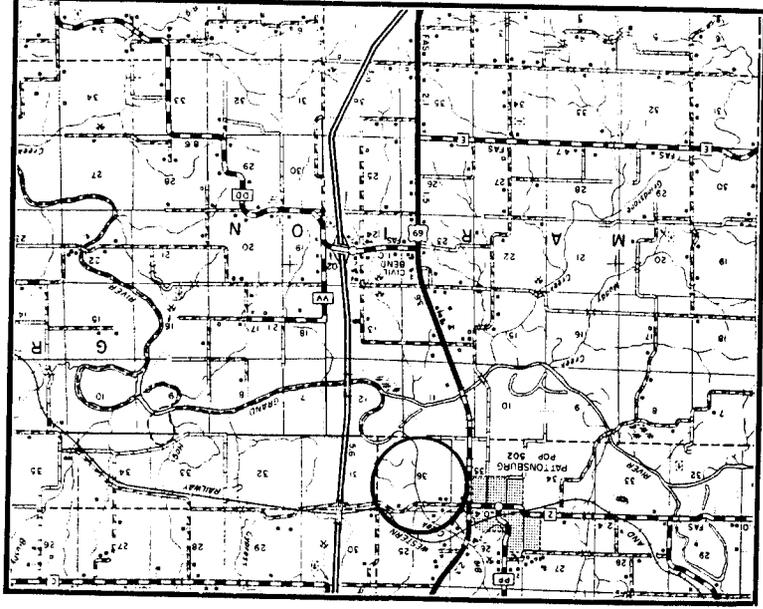
superstructure: steel, 8-panel, pin-connected Camelback through truss, with steel stringer approach span at the west end
substructure: concrete abutments, wingwalls and piers
floor/decking: concrete deck over steel stringers
other features: upper chord and inclined end post: 2 channels with cover plate and lacing; lower chord: 2 looped rectangular eyebars; vertical: 2 channels with lacing (4 angles with batten plates at the hip); diagonal: 2 looped rectangular eyebars; counter: round eyerods with turnbuckles; lateral bracing: round rod with threaded ends; strut: 2 angles; portal strut: 2 angles; guardrail: 2 angles (south side of west approach span is cable)

This single-span Camelback through truss over Big Creek features a 160-foot span length. Located in northwestern Daviess County on the edge of Pattonsburg, the Big Creek Bridge features a concrete deck over steel stringers and concrete abutments. The bridge construction company of choice in Daviess County, John Dildine Bridge Company of Cameron, Missouri, was probably contracted around 1910 to fabricate and erect the bridge. At some later time the original abutments were removed and new concrete abutments were installed. Other than this minor alteration, the structure remains structurally intact and continues to carry intermittent vehicular traffic.

Straight-chorded Pratt through trusses were used extensively throughout Missouri for medium-span crossings in the late 19th and early 20th centuries. For longer crossings after about the turn of the century, however, bridge companies could develop greater efficiency with polygonal-chorded Pratt variants - primarily Parker, Pennsylvania and Camelback trusses. With its distinctive five-faceted upper chords, the Camelback configuration was disdained by some engineers (including the redoubtable J.A.L. Waddell, who called it "uncompromisingly ugly") for its tendency under certain conditions to reverse compressive and tensile forces acting on their individual members. As a result, Camelback trusses never received widespread acceptance. Relatively few were ever built on Missouri's roads, and less than ten have been identified as extant by the bridge inventory. The Big Creek Bridge is thus technologically significant as a well-preserved example of this uncommon structural type.

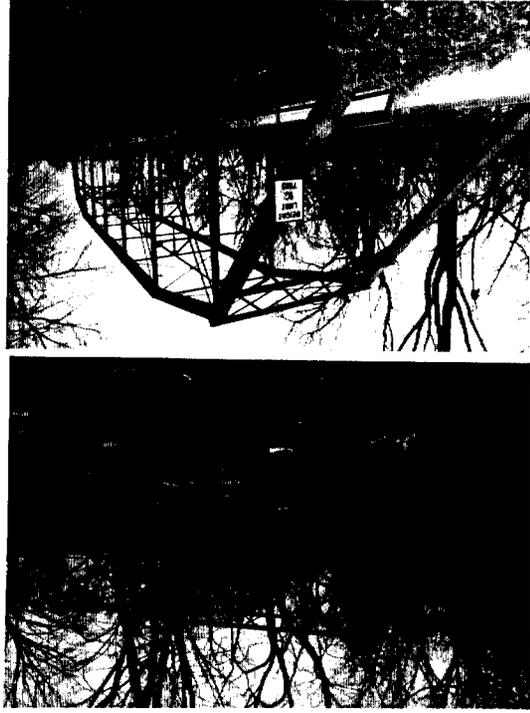
NAME(S) OF STRUCTURE
Big Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



LOCATION MAP

TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP



SOURCES

Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 359000.6; field inspection by Mitzi Rossillon, 9 September 1990.

INVENTORED BY
Michelle Crow-Dolby

AFFILIATION

Fraserdesign, Loveland CO

DATE

2 April 1993

HAER INVENTORY

Missouri Historic Bridge Inventory

NAME(S) OF STRUCTURE

Pilot Grove Creek Bridge
MHTD: 422R01.0

DAVI36

DATE(S) OF CONSTRUCTION

1947 (probable)

LOCATION

County Road 422 over Pilot Grove Creek; S28/33, T60N, R27W
3.5 miles southeast of Jameson; Daviess County, Missouri

USE (ORIGINAL / CURRENT)

roadway bridge / roadway bridge

RATING NRHP non-eligible (score: 37)

CONDITION

fair

OWNER

Daviess County

span number: 1
span length: 60.0'
total length: 114.0'
roadway wdt.: 15.5'

superstructure: steel, 4-panel, rigid-connected, 2-angle Pratt pony truss, with steel stringer approach spans
substructure: timber abutments; steel pile bent piers
floor/decking: timber deck over alternating steel and timber stringers
other features: upper chord and inclined end post: 2 angles; lower chord: 2 angles; vertical: 2 angles; diagonal: 2 angles; lateral bracing: round rod with threaded ends; guardrail: channel and angle

Designed by the Chillicothe Iron Works in Missouri and built by a county crew, this rigid-connected 2-angle Pratt pony truss dates to approximately 1947. The Pilot Grove Creek Bridge is situated in east-central Daviess County southeast of Jameson. The structure, which spans the Pilot Grove Creek, consists of a riveted Pratt pony truss, atypically configured with 2-angle upper chords. The bridge's abutments are not original, but the truss remains otherwise intact. Today, the bridge continues to carry intermittent traffic in its rural setting.

The oldest of the less than two dozen two-angle riveted Pratt pony trusses included in the historic bridge inventory, the poorly documented Pilot Grove Creek Bridge typifies this bridge building trend.

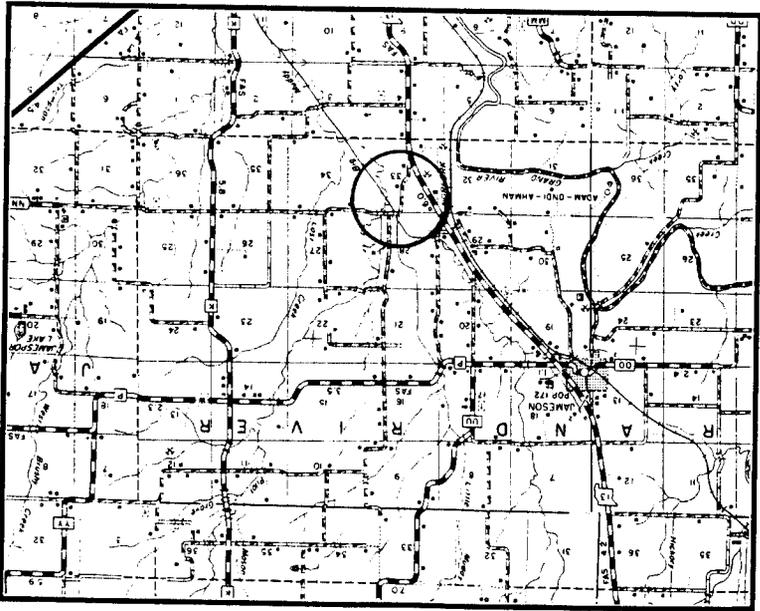
INVENTORIED BY
Michelle Crow-Dolby

AFFILIATION
Fraserdesign, Loveland CO

DATE
2 April 1993

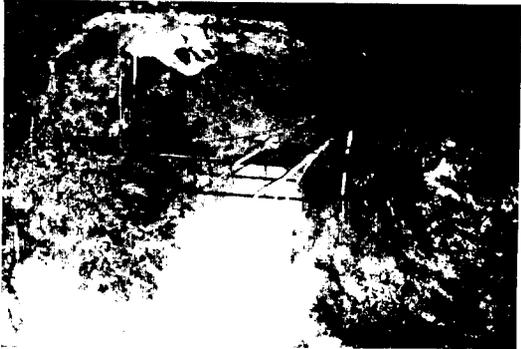
NAME(S) OF STRUCTURE
Pilot Grove Creek Bridge

PHOTOS AND SKETCH MAP OF LOCATION



TAKEN FROM MISSOURI HIGHWAY AND TRANSPORTATION DEPARTMENT
GENERAL HIGHWAY MAP

LOCATION MAP



SOURCES
Missouri Highway and Transportation Department, Structure Inventory and Appraisal: Structure Number 422R01.0; field inspection by
Mitzi Rossillon, 13 September 1990.